

# SEMAT I

## USS SIMPSON (FFG 56)

### SHELL CONDITION ASSESSMENT SURVEY INTERIOR AREAS

6/1/01 TO 6/8/01



**PREPARED BY  
SUPERVISOR OF SHIPBUILDING CONVERSION  
AND REPAIR  
PORTSMOUTH, VIRGINIA  
6/15/01**

**Q.E.D.**  
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12 June 2001

Q.E.D. Systems, Inc. was tasked by the Supervisor of Shipbuilding Conversion and Repair, USN (SPORT) Portsmouth, VA to conduct an Ultrasonic Thickness (U/T) Survey of specific locations on board USS SIMPSON (FFG 56) during the period of 1 June 2001 – 8 June 2001.

The survey was conducted by Francis J. (Joe) LaRock II, Certified Level I on 9 March 2001.

The instrument used was a Kraut Kramer Branson, Model DMS-2.

Serial Number: 00N3V8  
Calibration Date: 22 February 2001  
Probes Used: TC 560  
FH 2 E

I have reviewed the results of the survey and approved of the findings. The discrepancies have been recorded on MAF's and are included in this report.

  
Francis J. LaRock -  
Certified Level II  
9 March 2001

## SHELL CONDITION ASSESSMENT SURVEY SUMMARY

### Interior Areas

A Semat I Shell and Supporting Structure survey of all accessible interior areas was conducted on board the USS SIMPSON FFG 56 at NORSHIPCO in Norfolk VA. from 01 through 08, JUNE 2001.

The survey was conducted in accordance with Hull Structure MRC 1102 using a Delmhorst model BD 2100 moisture meter and a Krautkramer Branson model DM2TC ultrasonic test instrument, serial no. 00NV8 calibration due date; 02/22/02.

During the survey, areas historically prone to corrosion (hot spots) on FFG 7 class vessels were inspected and noted along with other areas where corrosion was found. The tank top underneath the air conditioning condenser in A/C Machinery Space (3-84-0-E) is the most critical area noted. The corrosion is due to inaccessibility between the bulkhead and the A/C unit, which has been noted as a class design problem.

The overall conditions of the USS SIMPSON were average or better than other FFG 7 class ships. Assessment of some historically prone "hot-spot" areas were unable to be conducted due to work being performed by the ship yard. The Survey Results section provides additional details.

# USS SIMPSON (FFG-56)

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**FFG-56 STRUCTURAL INSPECTION RESULTS**  
UNSATISFACTORY AREAS.

LOCATION	AREA	RESULTS
5-292-0-E AMR 3	PORT SIDE, BEHIND TLI RECEIVER AND AREA BELOW.	LIGHT SHELL PLATE AND STRINGER THINNING
5-292-0-E AMR 3	PORT, BILGE POCKET, FWD OF BHD 328	MARGIN PLATE THINNING
5-292-0-E AMR 3	PORT, TOP OF POTABLE WATER TANK (5-308-2-W)	APPROX 80 SF OF RUST AND PAINT SEPARATION
5-292-0-E AMR 3	CENTER LINE, TRANSVERSE WEB FRAME 316	APPROX 40 SF CORROSION AND PITTING AND PAINT SEPARATION
5-292-0-E AMR 3	STBD, TOP OF POTABLE WATER TANK (5-292-1-W)	APPROX 80 SF OF RUST AND PAINT SEPARATION, TOP IS THINNING.
5-292-0-E AMR 3	STBD SIDE, FROM WF (300-308) BILGE AREA.	APPROX 24 SF, SURFACE RUST AND PAINT SEPARATION.
5-250-0-E MER	PORT, FR (264-270), AT TRANSVERSE WELD SEAM BETWEEN STRINGERS 9-11.	SHELL IS THINNING AT WELD, HEAVY PITTING AND PAINT SEPARATION.
5-250-0-E MER	PORT, STRINGER 11, BETWEEN BHD (250) AND WF (258) UNDER THE EDUCTOR OBD MDV4 (3-255-2)	APPROX 40 SF OF SATURATED LAGGING, SURFACE RUST AND PAINT SEPARATION.
5-212-0-E AMR 2	PORT, FROM WF (220) TO WF (228) STRINGERS 7-12.	APPROX 144 SF OF SURFACE RUST AND PAINT SEPERATION.
5-212-0-E AMR 2	PORT SIDE, FROM WF (228-226).	APPROX 50 SF OF SURFACE RUST AND PAINT SEPARATION.
4-160-0-Q CHT	PORT BHD TANK TOP UNDER DRAIN FROM SEWAGE TRANSFER PUMP, INSIDE COAMING.	APPROX 6 SF OF SURFACE RUST AND PAINT SEPERATION.
4-140-0-Q LAUND	PORT SIDE, BILGE POCKET BETWEEN WF (148-164)	MARGIN PLATE THINNING, APPROX 48 SF OF LIGHT CORROSION AND PAINT SEPARATION.
4-172-1-E FPR	STBD SIDE, BILGE POCKET AND PUMP FOUNDATION.	APPROX 24SF OF SURFACE RUST AND PAINT SEPERATION.
3-84-0-E A/C MACH	PORT SIDE, DECK AND BHD 100	DECK AND BHD THINNING APPROX 40SF OF HEAVY CORROSION.
4-48-1-Q SNR CLG	STBD SIDE, BHD 56	APPROX 48 SF OF SATURATED LAGGING. BHD 56 HAS LIGHT CORROSION.

**FFG-56 STRUCTURAL INSPECTION RESULTS**  
SATISFACTORY AREAS.

LOCATION	AREA	RESULTS
3-32-1-K	FLAMMABLE LIQUID STORAGE	SAT
3-36-2-T	ESCAPE TRUNK	SAT
3-46-1-A	SPECIAL CLOTHING STOREROOM	SAT
3-53-2-A	STOREROOM	SAT
3-56-0-A	DECK GEAR STOREROOM	SAT
3-92-1-Q	CONVERTER ROOM	SAT
3-140-1-L	PASSAGEWAY	SAT
3-140-2-L	CREW LOUNGE	SAT
3-144-0-L	CREW BERTHING	SAT
3-154-1-Q	CREW WR, WC, & SHR	SAT
3-156-2-Q	CREW LOUNGE	SAT
3-180-2-C	SWITCH GEAR ROOM	SAT
3-180-5-A	DRY PROVISIONS STORE ROOM	SAT
3-196-2-A	SHIP'S STORE STOREROOM	SAT
3-328-0-A	SUPPLY STOREROOM	SAT
4-32-0-Q	SONAR EQUIPMENT ROOM	SAT
4-48-2-L	P-WAY	SAT
4-56-2-A	DECK STOWAGE	SAT
4-56-0-M	SMALL ARMS ROOM	SAT
4-100-0-E	APU MACHINERY ROOM	SAT
4-140-2-A	LAUNDRY	SAT
4-140-3-A	LAUNDRY STOREROOM	SAT
4-152-1-A	STOREROOM	SAT
5-56-0-M	SMALL ARMS MAGAZINE	SAT
5-180-0-E	AUXILIARY MACHINERY ROOM 1	SAT
5-368-01-E	AFTER STEERING	SAT
5-376-1-A	AFTER STEERING STOREROOM	SAT

**FFG-56 STRUCTURAL INSPECTION RESULTS  
UNASSESSABLE AREAS.**

LOCATION	AREA	RESULTS
3-20-0-Q	CHAIN LOCKER	UNASSESSABLE/REQUIRES GAS FREE
3-32-2-A	SECURITY FORCE ISSUE ROOM	UNASSESSABLE/REQUIRES GAS FREE
3-40-2-A	CPO STORE ROOM	UNASSESSABLE/FULL OF STORES
3-48-2-A	MEDICAL STOREROOM	UNASSESSABLE/REQUIRES GAS FREE
3-64-2-V	VOID	UNASSESSABLE/REQUIRES GAS FREE
3-64-1-V	VOID	UNASSESSABLE/REQUIRES GAS FREE
3-100-0-L	BERTHING SPACE	UNASSESSABLE/REQUIRES GAS FREE
3-100-1-L	CREW LOUNGE	UNASSESSABLE/REQUIRES GAS FREE
3-113-0-L	SANITARY SPACE	UNASSESSABLE/REQUIRES GAS FREE
3-236-1-F	FUEL TANK	UNASSESSABLE/REQUIRES GAS FREE
3-236-2-F	FUEL TANK	UNASSESSABLE/REQUIRES GAS FREE
3-240-1-F	FUEL TANK	UNASSESSABLE/REQUIRES GAS FREE
3-240-2-F	FUEL TANK	UNASSESSABLE/REQUIRES GAS FREE
3-272-1-F	FUEL TANK	UNASSESSABLE/REQUIRES GAS FREE
3-272-2-F	FUEL TANK	UNASSESSABLE/REQUIRES GAS FREE
3-278-1-F	FUEL TANK	UNASSESSABLE/REQUIRES GAS FREE
3-278-2-F	FUEL TANK	UNASSESSABLE/REQUIRES GAS FREE
3-286-1-F	FUEL TANK	UNASSESSABLE/REQUIRES GAS FREE
3-286-2-F	FUEL TANK	UNASSESSABLE/REQUIRES GAS FREE
3-292-8-F	FUEL TANK	UNASSESSABLE/REQUIRES GAS FREE
3-316-1-J	JP5 FUEL TANK	UNASSESSABLE/REQUIRES GAS FREE
3-322-1-J	JP5 FUEL TANK	UNASSESSABLE/REQUIRES GAS FREE
4-H-0-V	VOID	UNASSESSABLE/REQUIRES GAS FREE
4-27-0-V	VOID	UNASSESSABLE/REQUIRES GAS FREE
5-FF-0-V	VOID	UNASSESSABLE/REQUIRES GAS FREE
5-32-0-W	SALT WATER TANK	UNASSESSABLE/REQUIRES GAS FREE
5-51-0-Q	EDUCTOR ROOM	UNASSESSABLE
5-56-0-F	FUEL TANK	UNASSESSABLE/REQUIRES GAS FREE
5-64-0-F	FUEL TANK	UNASSESSABLE/REQUIRES GAS FREE
5-84-1-F	FUEL TANK	UNASSESSABLE/REQUIRES GAS FREE
5-84-2-F	FUEL TANK	UNASSESSABLE/REQUIRES GAS FREE
5-100-1-W	APU WELL	UNASSESSABLE
5-100-2-W	APU WELL	UNASSESSABLE
5-100-3-F	FUEL TANK	UNASSESSABLE/REQUIRES GAS FREE
5-100-4-F	FUEL TANK	UNASSESSABLE/REQUIRES GAS FREE
5-116-1-F	FUEL TANK	UNASSESSABLE/REQUIRES GAS FREE
5-116-2-F	FUEL TANK	UNASSESSABLE/REQUIRES GAS FREE
5½-116-0-V	VOID	UNASSESSABLE/REQUIRES GAS FREE
5-132-0-F	CONTAMINATED OIL TANK	UNASSESSABLE/REQUIRES GAS FREE
5-137-1-T	UNDERWATER LOG TANK	UNASSESSABLE/REQUIRES GAS FREE
5-140-1-F	FUEL TANK	UNASSESSABLE/REQUIRES GAS FREE
5-140-2-F	FUEL TANK	UNASSESSABLE/REQUIRES GAS FREE
5-164-0-F	CONTAMINATED OIL TANK	UNASSESSABLE/REQUIRES GAS FREE
5-164-1-F	FUEL TANK	UNASSESSABLE/REQUIRES GAS FREE
5-164-2-F	FUEL TANK	UNASSESSABLE/REQUIRES GAS FREE

5-170-0-F	CONTAMINATED OIL TANK	UNASSESSABLE/REQUIRES GAS FREE
5-192-2-F	FUEL TANK	UNASSESSABLE/REQUIRES GAS FREE
5-201-1-F	FUEL TANK	UNASSESSABLE/REQUIRES GAS FREE
5-201-3-F	FUEL TANK	UNASSESSABLE/REQUIRES GAS FREE
5-204-1-F	FUEL TANK	UNASSESSABLE/REQUIRES GAS FREE
5-204-2-F	FUEL TANK	UNASSESSABLE/REQUIRES GAS FREE
5-220-2-W	WASTE STORAGE TANK	UNASSESSABLE/REQUIRES GAS FREE
5-222-1-F	FUEL TANK	UNASSESSABLE/REQUIRES GAS FREE
5-250-1-F	FUEL TANK	UNASSESSABLE/REQUIRES GAS FREE
5-250-2-F	FUEL TANK	UNASSESSABLE/REQUIRES GAS FREE
5-270-0-F	HYDRAULIC OIL TANK	UNASSESSABLE/REQUIRES GAS FREE
5-276-0-F	LUBE OIL TANK	UNASSESSABLE/REQUIRES GAS FREE
5-290-2-F	FUEL TANK	UNASSESSABLE/REQUIRES GAS FREE

# MATERIAL ASSESSMENT FORM

ITEM NUMBER				SHIP <b>USS SIMPSON FFG 56</b>				
SYSTEM Hull Structure		ESWS 11011	EIC A100	APL X-SYSTEMS		RIN N/L		
EQUIPMENT NAME Shell & Supporting Structure		IDENT/SERIAL N/A		LOCATION 5-292-0-E	WORK CENTER EMO2	JSN P050		
EQUIPMENT STATUS		PROBLEM STATUS		CATEGORY			EOC	
1. SAT - SATISFACTORY 2. <b>UNSAT - UNSATISFACTORY</b> 3. TNA - TEST NOT ATTEMPTED 4. TNC - TEST NOT COMPLETED 5. NOB - EQUIPMENT NOT ONBOARD		1. C - CORRECTED 2. A - AWAITING PARTS 3. T - AWAITING TECH ASSIST* 4. S - SHIP'S FORCE * <b>UNCORRECTED PROBLEMS</b>		1. S - PERSONNEL SAFETY 2. Z - EQUIPMENT SAFETY 3. <b>H - HARDWARE</b> 4. D - DOWN 5. L - LIMITED			<b>.9</b>	
TYPE/AVAILABILITY	WHEN TO BE ACCOMPLISHED	STATUS		CAUSE		DEFERRAL REASON		
1. DEPOT 2. IMA 3. <b>TECH ASSIST</b> 4. SHIP'S FORCE 5. SFWL	1. IMMEDIATELY 2. PRIOR TO NEXT UNDERWAY 3. PRIOR TO NEXT DEPLOYMENT 4. <b>DURING POST DEPLOYMENT AVAILABILITY</b>	1. <b>OPERATIONAL</b> 2. NON-OPERATIONAL 3. REDUCED CAPABILITY 0. NOT APPLICABLE		1. ABNORMAL ENVIRONMENT 2. MANUFACTURER/INSTALLATION DEFECTS 3. LACK OF KNOWLEDGE 4. COMMUNICATION PROBLEMS 5. INADEQUATE INSTRUCTION/PROCEDURE 6. INADEQUATE DESIGN 7. <b>NORMAL WEAR AND TEAR</b> 0. NOT APPLICABLE		1. S/F BACKLOG/OPERATIONAL PRIORITY 2. LACK OF MATERIAL 3. NO FORMAL TRAINING ON THIS EQUIPMENT 4. FORMAL TRAINING INADEQUATE IN THIS EQUIPMENT 5. INADEQUATE SCHOOL PRACTICAL TRAINING 6. LACK OF FACILITIES/CAPABILITIES 7. <b>NOT AUTHORIZED FOR S/F ACCOMPLISHMENT</b> 8. FOR S/F OVERHAUL OR AVAILABILITY WORK LIST 9. LACK OF TECHNICAL DOCUMENTATION 0. OTHER OR NOT APPLICABLE		
MAN HRS EXPENDED 2	MAN HRS REMAINING 600		COMPLETED ACTION TAKEN		S/F MANHOURS	PRIORITY		
<b>SAFETY HAZARD:</b> 1. SERIOUS - CORRECT AS SOON AS POSSIBLE 2. SERIOUS - SUSPENSION OF EQUIPMENT/SYSTEM/SPACE REQUIRED 3. SERIOUS - WAIVER OF EQUIPMENT/SYSTEM 4. <b>SAFETY ITEM - MINOR</b> 5. COMBUSTIBLE MATERIAL			1. MAINTENANCE ACTION COMPLETED: PARTS DRAWN FROM SUPPLY 2. MAINTENANCE ACTION COMPLETED: REQUIRED PARTS NOT DRAWN FROM SUPPLY 3. MAINTENANCE ACTION COMPLETED: NO PARTS REQUIRED 4. CANCELLED - DEFERRAL WILL BE REMOVED FROM CSMP 7. <b>MAINTENANCE ACTION COMPLETED: 3-M CAPABILITY UTILIZED</b> 0. NONE OF THE ABOVE, DESCRIBE IN REMARKS/DESCRIPTION SECTION			1. MANDATORY 2. ESSENTIAL 3. HIGHLY DESIRABLE 4. <b>DESIRABLE</b>		
DISCREPANCY DESCRIPTION: PER SEMAT I INSPECTION								
Port side, (5-292-0-E) between WF(316-324), under the overboard discharge between stringers (10-11), the shell plate is thinning at <b>.287, .253, .251</b> , the shell between stringers (9-10) has shell thinning at <b>.296 &amp; .307</b> . behind the tank level indicator receiver, the shell is thinned at <b>.295</b> and The web on stringer (13) is thinned at <b>.190</b> , and the web at stringer (12) is thinned at <b>.193</b> , and the web at stringer (10) is thinned at <b>.190</b> .								
RECOMMEND REPAIR ACTIVITY: Replace approximately 24 linear feet of 6x4 T-beam and approximately 64sf of shell plate with .313 steel, preserve and paint to match area. FTSC IS TO CONDUCT RISK ASSESSMENT AND MAKE FINAL REPAIR RECOMMENDATIONS								
SSPORT C221 POC: F. PRAISSMAN (757) 396-4001 EXT 2130, TPOC: T. ZWOLAK								
CSMP SUMMARY: SHELL AND STRINGERS THINNING.					TEST# MRC 1102/1			
ROOT CAUSE/AMPLIFICATION: H1					STEP# 1,2,3,4 & 5			
SYSTEM LEVEL IMPACT:								
PART NO.	NOMEMCLATURE			QTY	NSN		COST	
1								
2								
3								
4								
5								
6								
FIRST CONTACT LARSEN	RATE EN3	SECOND CONTACT LARSEN		RATE EN3	PHONE 22			
ASSESSOR / ACT / TECH ID# J. LaROCK / T. ILGENFRITZ QED SYSTEMS				TD	TL	LOGISTICIAN	TSP	

**AUXILIARY MACHINERY ROOM 3 (5-292-0-E)**



AREA VIEW, PORT SIDE  
ARROW INDICATES TLI RECEIVER



OVERBOARD DISCHARGE BELOW TLI RECEIVER  
U/T READINGS ARE NOTED IN DRAWING  
AREA RECOMMENDED FOR PRESERVATION

**AUXILIARY MACHINERY ROOM 3 (5-292-0-E)**



AREA VIEW, PORT SIDE  
ARROW INDICATES TLI RECEIVER

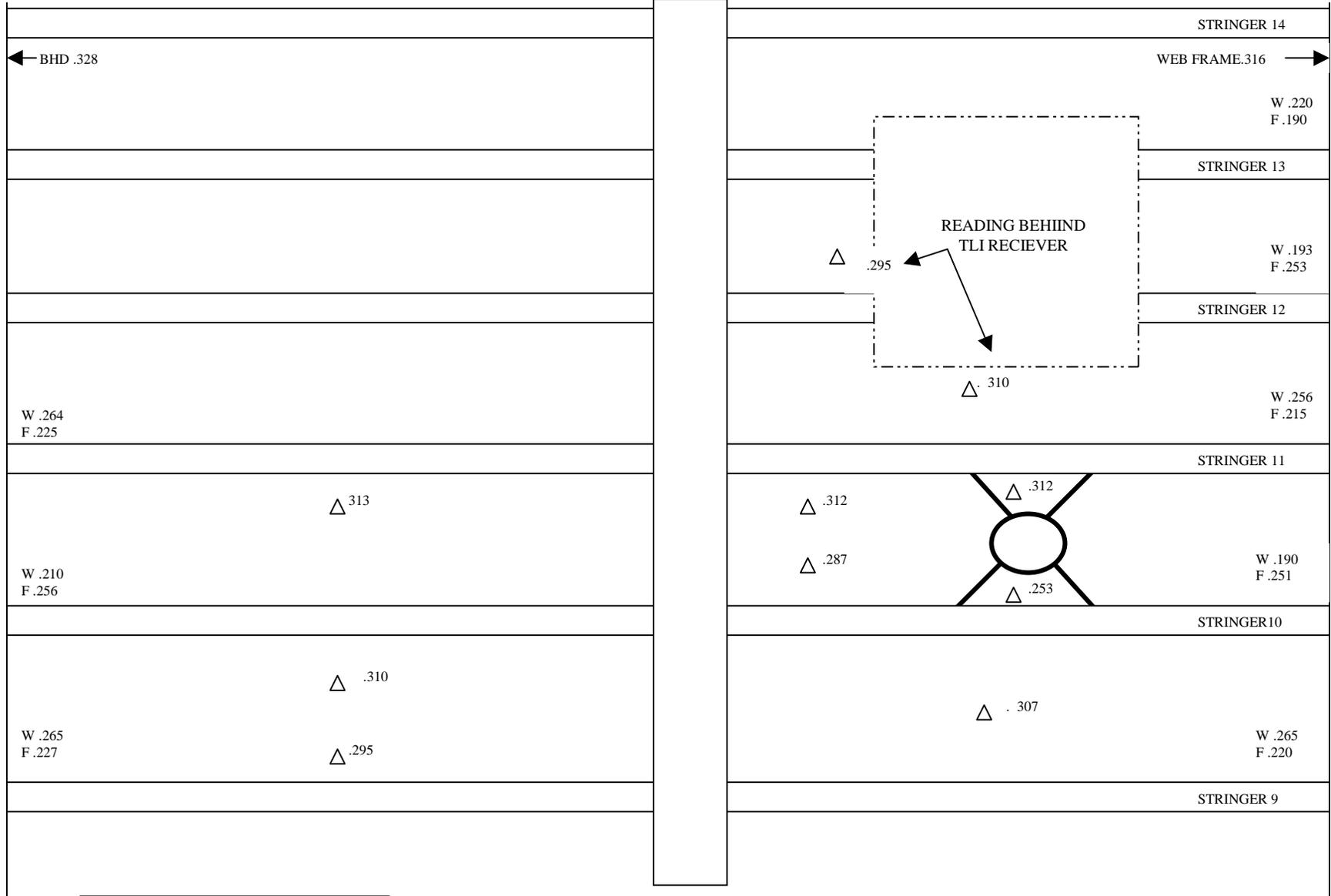


STRINGER 9 AND AREA BELOW  
U/T READINGS ARE NOTED IN DRAWING  
AREA RECOMMENDED FOR PRESERVATION

W .220  
F .265

WEB FRAME  
.324

W .222  
F .372



△	. SHELL PLATE
W	STRINGER WEB
F	STRINGER FLANGE

USS SIMPSON (FFG 56)  
AUXILLIARY MACH RM SSDG #4  
(5-292-0-E) PORT SIDE

J. LaROCK / T. ILGENFRITZ  
QED SYSTEMS INC  
06/08/01

# MATERIAL ASSESSMENT FORM

ITEM NUMBER				SHIP <b>USS SIMPSON FFG 56</b>			
SYSTEM Hull Structure		ESWBS 11011	EIC A100	APL X-SYSTEMS		RIN N/L	
EQUIPMENT NAME Shell & Supporting Structure		IDENT/SERIAL N/A		LOCATION 5-292-0-E	WORK CENTER EMO2	JSN P048	
EQUIPMENT STATUS		PROBLEM STATUS		CATEGORY			EOC
1. SAT - SATISFACTORY 2. <b>UNSAT - UNSATISFACTORY</b> 3. TNA - TEST NOT ATTEMPTED 4. TNC - TEST NOT COMPLETED 5. NOB - EQUIPMENT NOT ONBOARD		1. C - CORRECTED 2. A - AWAITING PARTS 3. T - AWAITING TECH ASSIST* 4. <b>S - SHIP'S FORCE</b> *UNCORRECTED PROBLEMS		1. S - PERSONNEL SAFETY 2. Z - EQUIPMENT SAFETY 3. <b>H - HARDWARE</b>		4. D - DOWN 5. L - LIMITED  <b>.9</b>	
TYPE/AVAILABILITY	WHEN TO BE ACCOMPLISHED	STATUS	CAUSE		DEFERRAL REASON		
1. DEPOT 2. IMA 3. <b>TECH ASSIST</b> 4. SHIP'S FORCE 5. SFWL	1. IMMEDIATELY 2. PRIOR TO NEXT UNDERWAY 3. PRIOR TO NEXT DEPLOYMENT 4. <b>DURING POST DEPLOYMENT AVAILABILITY</b>	1. <b>OPERATIONAL</b> 2. NON-OPERATIONAL 3. REDUCED CAPABILITY 0. NOT APPLICABLE	1. ABNORMAL ENVIRONMENT 2. MANUFACTURER/INSTALLATION DEFECTS 3. LACK OF KNOWLEDGE 4. COMMUNICATION PROBLEMS 5. INADEQUATE INSTRUCTION/PROCEDURE 6. INADEQUATE DESIGN 7. <b>NORMAL WEAR AND TEAR</b> 0. NOT APPLICABLE		1. <b>S/F BACKLOG/OPERATIONAL PRIORITY</b> 2. LACK OF MATERIAL 3. NO FORMAL TRAINING ON THIS EQUIPMENT 4. FORMAL TRAINING INADEQUATE IN THIS EQUIPMENT 5. INADEQUATE SCHOOL PRACTICAL TRAINING 6. LACK OF FACILITIES/CAPABILITIES 7. NOT AUTHORIZED FOR S/F ACCOMPLISHMENT 8. FOR S/F OVERHAUL OR AVAILABILITY WORK LIST 9. LACK OF TECHNICAL DOCUMENTATION 0. OTHER OR NOT APPLICABLE		
MAN HRS EXPENDED 2	MAN HRS REMAINING 100	COMPLETED ACTION TAKEN		S/F MANHOURS	PRIORITY		
<b>SAFETY HAZARD:</b> 1. SERIOUS - CORRECT AS SOON AS POSSIBLE 2. SERIOUS - SUSPENSION OF EQUIPMENT/SYSTEM/SPACE REQUIRED 3. SERIOUS - WAIVER OF EQUIPMENT/SYSTEM 4. <b>SAFETY ITEM - MINOR</b> 5. COMBUSTIBLE MATERIAL		1. MAINTENANCE ACTION COMPLETED: PARTS DRAWN FROM SUPPLY 2. MAINTENANCE ACTION COMPLETED: REQUIRED PARTS NOT DRAWN FROM SUPPLY 3. MAINTENANCE ACTION COMPLETED: NO PARTS REQUIRED 4. CANCELLED - DEFERRAL WILL BE REMOVED FROM CSMP 7. <b>MAINTENANCE ACTION COMPLETED: 3-M CAPABILITY UTILIZED</b> 0. NONE OF THE ABOVE, DESCRIBE IN REMARKS/DESCRIPTION SECTION			1. MANDATORY 2. ESSENTIAL 3. HIGHLY DESIRABLE 4. <b>DESIRABLE</b>		
DISCREPANCY DESCRIPTION: PER SEMAT I INSPECTION							
Port side (5-292-0-E), 6" fwd of BHD (328), 4" up from the shell on the margin plate has a low U/T reading at <b>.201</b> , 10" above, the reading is <b>.252</b> .							
RECOMMEND REPAIR ACTIVITY: Cut out and replace approximately 6sf of margin plate with .250 steel. Preserve and paint to match area. FTSC is to conduct risk assessment and make final repair recommendations.							
SSPORT C221 POC: F. PRAISSMAN (757) 396-4001 EXT 2130, TPOC: T. ZWOLAK							
CSMP SUMMARY: Margin plate is thinning.					TEST# MRC 1102/1		
ROOT CAUSE/AMPLIFICATION: H1					STEP# 1,2,3,4 & 5		
SYSTEM LEVEL IMPACT:							
PART NO.	NOMEMCLATURE			QTY	NSN	COST	
1							
2							
3							
4							
5							
6							
FIRST CONTACT LARSEN	RATE EN3	SECOND CONTACT LARSEN		RATE EN3	PHONE 22		
ASSESSOR / ACT / TECH ID# J. LaROCK / T. ILGENFRITZ QED SYSTEMS				TD	TL	LOGISTICIAN	TSP

**AUXILIARY MACHINERY ROOM 3 (5-292-0-E)**



PORT SIDE, BILGE POCKET FORWARD OF BHD 328  
ARROW INDICATES U/T READING OF .201



SHELL PLATE ABOVE STRINGER 8  
AREA RECOMMENDED FOR PRESERVATION

# MATERIAL ASSESSMENT FORM

ITEM NUMBER				SHIP <b>USS SIMPSON FFG 56</b>				
SYSTEM Hull Structure		ESWBS 11011	EIC A100	APL X-SYSTEMS		RIN N/L		
EQUIPMENT NAME Shell & Supporting Structure		IDENT/SERIAL N/A		LOCATION 5-292-0-E	WORK CENTER EMO2	JSN P046		
EQUIPMENT STATUS		PROBLEM STATUS		CATEGORY			EOC	
1. SAT - SATISFACTORY 2. <b>UNSAT - UNSATISFACTORY</b> 3. TNA - TEST NOT ATTEMPTED 4. TNC - TEST NOT COMPLETED 5. NOB - EQUIPMENT NOT ONBOARD		1. C - CORRECTED 2. A - AWAITING PARTS 3. T - AWAITING TECH ASSIST* 4. <b>S - SHIP'S FORCE</b> *UNCORRECTED PROBLEMS		1. S - PERSONNEL SAFETY 2. Z - EQUIPMENT SAFETY 3. <b>H - HARDWARE</b> 4. D - DOWN 5. L - LIMITED			<b>.9</b>	
TYPE/AVAILABILITY	WHEN TO BE ACCOMPLISHED	STATUS	CAUSE		DEFERRAL REASON			
1. DEPOT 2. IMA 3. TECH ASSIST 4. <b>SHIP'S FORCE</b> 5. SFWL	1. IMMEDIATELY 2. PRIOR TO NEXT UNDERWAY 3. PRIOR TO NEXT DEPLOYMENT 4. <b>DURING POST DEPLOYMENT AVAILABILITY</b>	1. <b>OPERATIONAL</b> 2. NON-OPERATIONAL 3. REDUCED CAPABILITY 0. NOT APPLICABLE	1. ABNORMAL ENVIRONMENT 2. MANUFACTURER/INSTALLATION DEFECTS 3. LACK OF KNOWLEDGE 4. COMMUNICATION PROBLEMS 5. INADEQUATE INSTRUCTION/PROCEDURE 6. INADEQUATE DESIGN 7. <b>NORMAL WEAR AND TEAR</b> 0. NOT APPLICABLE		1. <b>S/F BACKLOG/OPERATIONAL PRIORITY</b> 2. LACK OF MATERIAL 3. NO FORMAL TRAINING ON THIS EQUIPMENT 4. FORMAL TRAINING INADEQUATE IN THIS EQUIPMENT 5. INADEQUATE SCHOOL PRACTICAL TRAINING 6. LACK OF FACILITIES/CAPABILITIES 7. NOT AUTHORIZED FOR S/F ACCOMPLISHMENT 8. FOR S/F OVERHAUL OR AVAILABILITY WORK LIST 9. LACK OF TECHNICAL DOCUMENTATION 0. OTHER OR NOT APPLICABLE			
MAN HRS EXPENDED 2	MAN HRS REMAINING 100	COMPLETED ACTION TAKEN		S/F MANHOURS	PRIORITY			
<b>SAFETY HAZARD:</b> 1. SERIOUS - CORRECT AS SOON AS POSSIBLE 2. SERIOUS - SUSPENSION OF EQUIPMENT/SYSTEM/SPACE REQUIRED 3. SERIOUS - WAIVER OF EQUIPMENT/SYSTEM 4. <b>SAFETY ITEM - MINOR</b> 5. COMBUSTIBLE MATERIAL		1. MAINTENANCE ACTION COMPLETED: PARTS DRAWN FROM SUPPLY 2. MAINTENANCE ACTION COMPLETED: REQUIRED PARTS NOT DRAWN FROM SUPPLY 3. MAINTENANCE ACTION COMPLETED: NO PARTS REQUIRED 4. CANCELLED - DEFERRAL WILL BE REMOVED FROM CSMP 7. <b>MAINTENANCE ACTION COMPLETED: 3-M CAPABILITY UTILIZED</b> 0. NONE OF THE ABOVE, DESCRIBE IN REMARKS/DESCRIPTION SECTION			1. MANDATORY 2. ESSENTIAL 3. HIGHLY DESIRABLE 4. <b>DESIRABLE</b>			
DISCREPANCY DESCRIPTION: PER SEMAT I INSPECTION								
Port side (5-292-0-E), top of potable water tank (5-308-2), has light surface rust and paint separation, approximately 80sf.								
<b>RECOMMENDED REPAIRS:</b> Needle-gun approximately 80sf of tank top, preserve and paint to match area.								
SSPORT C221 POC: F. PRAISSMAN (757) 396-4001 EXT 2130, TPOC: T. ZWOLAK								
CSMP SUMMARY: Potable water tank top needs preservation.					TEST# MRC 1102/1			
ROOT CAUSE/AMPLIFICATION: H1					STEP# 1,2,3,4 & 5			
SYSTEM LEVEL IMPACT:								
PART NO.	NOMEMCLATURE			QTY	NSN		COST	
1								
2								
3								
4								
5								
6								
FIRST CONTACT LARSEN	RATE EN3	SECOND CONTACT LARSEN		RATE EN3	PHONE 22			
ASSESSOR / ACT / TECH ID# J. LaROCK / T. ILGENFRITZ QED SYSTEMS				TD	TL	LOGISTICIAN	TSP	

**AUXILIARY MACHINERY ROOM 3 (5-292-0-E)**



TOP OF POTABLE WATER TANK (5-308-2)



PORT SIDE, TOP OF POTABLE WATER TANK (5-308-2)  
AREA RECOMMENDED FOR PRESERVATION

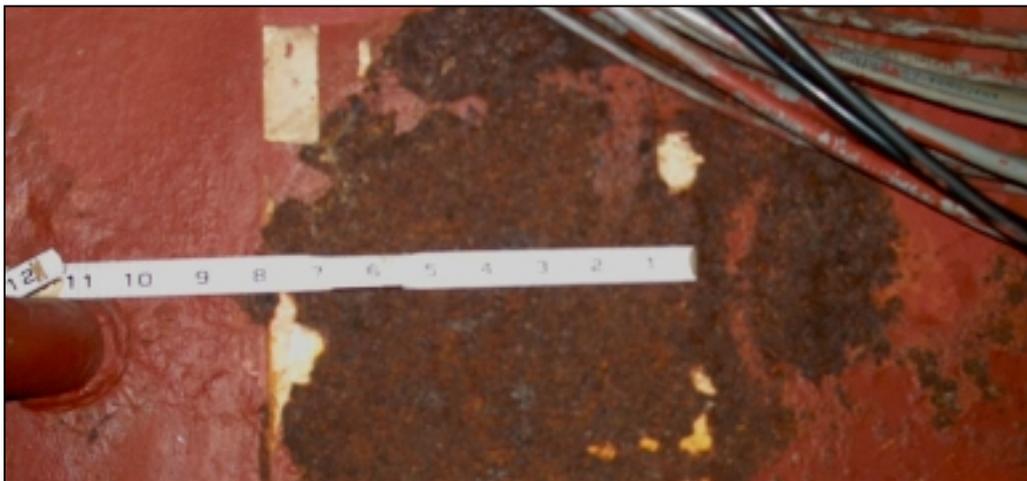
# MATERIAL ASSESSMENT FORM

ITEM NUMBER				SHIP <b>USS SIMPSON FFG 56</b>				
SYSTEM Hull Structure		ESWBS 11011	EIC A100	APL X-SYSTEMS		RIN N/L		
EQUIPMENT NAME Shell & Supporting Structure		IDENT/SERIAL N/A		LOCATION 5-292-0-E	WORK CENTER EMO2	JSN P051		
EQUIPMENT STATUS		PROBLEM STATUS		CATEGORY			EOC	
1. SAT - SATISFACTORY 2. <b>UNSAT - UNSATISFACTORY</b> 3. TNA - TEST NOT ATTEMPTED 4. TNC - TEST NOT COMPLETED 5. NOB - EQUIPMENT NOT ONBOARD		1. C - CORRECTED 2. A - AWAITING PARTS 3. T - AWAITING TECH ASSIST* 4. <b>S - SHIP'S FORCE</b> *UNCORRECTED PROBLEMS		1. S - PERSONNEL SAFETY 2. Z - EQUIPMENT SAFETY 3. <b>H - HARDWARE</b> 4. D - DOWN 5. L - LIMITED			<b>.9</b>	
TYPE/AVAILABILITY	WHEN TO BE ACCOMPLISHED	STATUS	CAUSE		DEFERRAL REASON			
1. DEPOT 2. IMA 3. TECH ASSIST 4. <b>SHIP'S FORCE</b> 5. SFWL	1. IMMEDIATELY 2. PRIOR TO NEXT UNDERWAY 3. PRIOR TO NEXT DEPLOYMENT 4. <b>DURING POST DEPLOYMENT AVAILABILITY</b>	1. <b>OPERATIONAL</b> 2. NON-OPERATIONAL 3. REDUCED CAPABILITY 0. NOT APPLICABLE	1. ABNORMAL ENVIRONMENT 2. MANUFACTURER/INSTALLATION DEFECTS 3. LACK OF KNOWLEDGE 4. COMMUNICATION PROBLEMS 5. INADEQUATE INSTRUCTION/PROCEDURE 6. INADEQUATE DESIGN 7. <b>NORMAL WEAR AND TEAR</b> 0. NOT APPLICABLE		1. <b>S/F BACKLOG/OPERATIONAL PRIORITY</b> 2. LACK OF MATERIAL 3. NO FORMAL TRAINING ON THIS EQUIPMENT 4. FORMAL TRAINING INADEQUATE IN THIS EQUIPMENT 5. INADEQUATE SCHOOL PRACTICAL TRAINING 6. LACK OF FACILITIES/CAPABILITIES 7. NOT AUTHORIZED FOR S/F ACCOMPLISHMENT 8. FOR S/F OVERHAUL OR AVAILABILITY WORK LIST 9. LACK OF TECHNICAL DOCUMENTATION 0. OTHER OR NOT APPLICABLE			
MAN HRS EXPENDED 2	MAN HRS REMAINING 100	COMPLETED ACTION TAKEN		S/F MANHOURS	PRIORITY			
<b>SAFETY HAZARD:</b> 1. SERIOUS - CORRECT AS SOON AS POSSIBLE 2. SERIOUS - SUSPENSION OF EQUIPMENT/SYSTEM/SPACE REQUIRED 3. SERIOUS - WAIVER OF EQUIPMENT/SYSTEM 4. <b>SAFETY ITEM - MINOR</b> 5. COMBUSTIBLE MATERIAL		1. MAINTENANCE ACTION COMPLETED: PARTS DRAWN FROM SUPPLY 2. MAINTENANCE ACTION COMPLETED: REQUIRED PARTS NOT DRAWN FROM SUPPLY 3. MAINTENANCE ACTION COMPLETED: NO PARTS REQUIRED 4. CANCELLED - DEFERRAL WILL BE REMOVED FROM CSMP 7. <b>MAINTENANCE ACTION COMPLETED: 3-M CAPABILITY UTILIZED</b> 0. NONE OF THE ABOVE, DESCRIBE IN REMARKS/DESCRIPTION SECTION			1. MANDATORY 2. ESSENTIAL 3. HIGHLY DESIRABLE 4. <b>DESIRABLE</b>			
DISCREPANCY DESCRIPTION: PER SEMAT I INSPECTION (5-292-0-E), Transverse WF (316) has heavy pitting, heavy rust and heavy paint separation. U/T readings are satisfactory approximately 40sf.								
RECOMMENDED REPAIRS: Needle gun approximately 40sf of transverse WF (316), preserve and paint to match.								
SSPORT C221 POC: F. PRAISSMAN (757) 396-4001 EXT 2130, TPOC: T. ZWOLAK								
CSMP SUMMARY: Transverse WF (316) showing signs of deterioration.					TEST# MRC 1102/1			
ROOT CAUSE/AMPLIFICATION: H1					STEP# 1,2,3,4 & 5			
SYSTEM LEVEL IMPACT:								
PART NO.	NOMENCLATURE			QTY	NSN		COST	
1								
2								
3								
4								
5								
6								
FIRST CONTACT LARSEN	RATE EN3	SECOND CONTACT LARSEN		RATE EN3	PHONE 22			
ASSESSOR / ACT / TECH ID# J. LaROCK / T. ILGENFRITZ QED SYSTEMS				TD	TL	LOGISTICIAN	TSP	

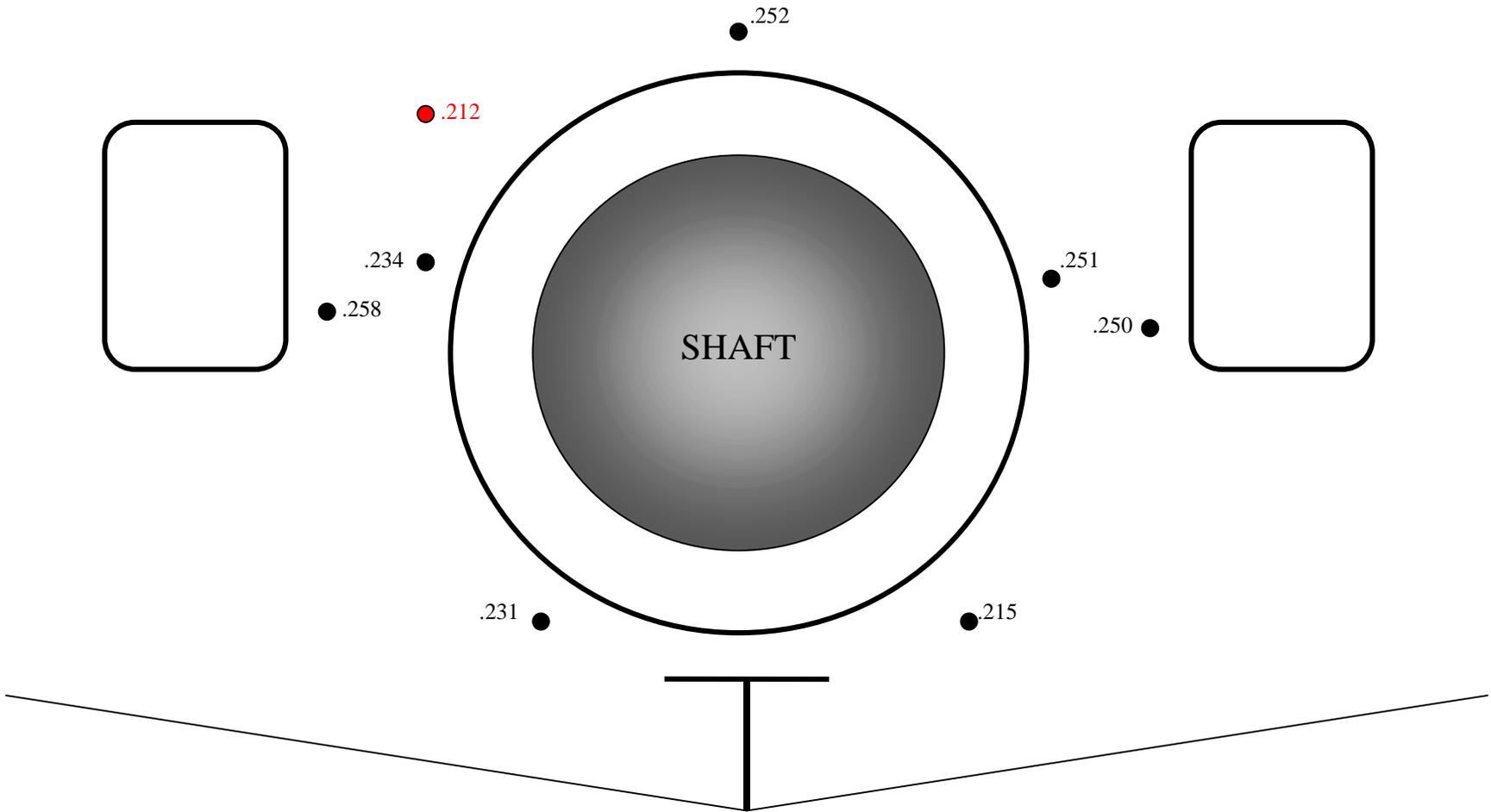
**AUXILIARY MACHINERY ROOM 3 (5-292-0-E)**



STBD SIDE, TRANSVERSE WEB FRAM 316  
U/T READINGS NOTED IN DRAWING.



PORT SIDE, TRANSVERSE WEB 316  
AREA WITH HEAVY PITTING.



TRANSVERSE WEB FRAME 316  
LOOKING FORWARD

USS SIMPSON (FFG 56)  
AUXILIARY ROOM #3  
(5-292-0-E)

J. LaROCK / T. ILGENFRITZ  
QED SYSTEMS INC.  
06/09/01

# MATERIAL ASSESSMENT FORM

ITEM NUMBER				SHIP <b>USS SIMPSON FFG 56</b>			
SYSTEM Hull Structure		ESWBS 11011	EIC A100	APL X-SYSTEMS		RIN N/L	
EQUIPMENT NAME Shell & Supporting Structure		IDENT/SERIAL N/A		LOCATION 5-292-0-E	WORK CENTER EMO2	JSN P042	
EQUIPMENT STATUS		PROBLEM STATUS		CATEGORY			EOC
1. SAT - SATISFACTORY 2. <b>UNSAT - UNSATISFACTORY</b> 3. TNA - TEST NOT ATTEMPTED 4. TNC - TEST NOT COMPLETED 5. NOB - EQUIPMENT NOT ONBOARD		1. C - CORRECTED 2. A - AWAITING PARTS 3. <b>T - AWAITING TECH ASSIST*</b> 4. S - SHIP'S FORCE *UNCORRECTED PROBLEMS		1. S - PERSONNEL SAFETY 2. Z - EQUIPMENT SAFETY 3. <b>H - HARDWARE</b>		4. D - DOWN 5. L - LIMITED  <b>.9</b>	
TYPE/AVAILABILITY	WHEN TO BE ACCOMPLISHED	STATUS	CAUSE		DEFERRAL REASON		
1. DEPOT 2. IMA 3. <b>TECH ASSIST</b> 4. SHIP'S FORCE 5. SFWL	1. IMMEDIATELY 2. PRIOR TO NEXT UNDERWAY 3. PRIOR TO NEXT DEPLOYMENT 4. <b>DURING POST DEPLOYMENT AVAILABILITY</b>	1. <b>OPERATIONAL</b> 2. NON-OPERATIONAL 3. REDUCED CAPABILITY 0. NOT APPLICABLE	1. ABNORMAL ENVIRONMENT 2. MANUFACTURER/INSTALLATION DEFECTS 3. LACK OF KNOWLEDGE 4. COMMUNICATION PROBLEMS 5. INADEQUATE INSTRUCTION/PROCEDURE 6. INADEQUATE DESIGN 7. <b>NORMAL WEAR AND TEAR</b> 0. NOT APPLICABLE		1. S/F BACKLOG/OPERATIONAL PRIORITY 2. LACK OF MATERIAL 3. NO FORMAL TRAINING ON THIS EQUIPMENT 4. FORMAL TRAINING INADEQUATE IN THIS EQUIPMENT 5. INADEQUATE SCHOOL PRACTICAL TRAINING 6. LACK OF FACILITIES/CAPABILITIES 7. <b>NOT AUTHORIZED FOR S/F ACCOMPLISHMENT</b> 8. FOR S/F OVERHAUL OR AVAILABILITY WORK LIST 9. LACK OF TECHNICAL DOCUMENTATION 0. OTHER OR NOT APPLICABLE		
MAN HRS EXPENDED 2	MAN HRS REMAINING 200	COMPLETED ACTION TAKEN		S/F MANHOURS	PRIORITY		
<b>SAFETY HAZARD:</b> 1. SERIOUS - CORRECT AS SOON AS POSSIBLE 2. SERIOUS - SUSPENSION OF EQUIPMENT/SYSTEM/SPACE REQUIRED 3. SERIOUS - WAIVER OF EQUIPMENT/SYSTEM 4. <b>SAFETY ITEM - MINOR</b> 5. COMBUSTIBLE MATERIAL		1. MAINTENANCE ACTION COMPLETED: PARTS DRAWN FROM SUPPLY 2. MAINTENANCE ACTION COMPLETED: REQUIRED PARTS NOT DRAWN FROM SUPPLY 3. MAINTENANCE ACTION COMPLETED: NO PARTS REQUIRED 4. CANCELLED - DEFERRAL WILL BE REMOVED FROM CSMP 7. <b>MAINTENANCE ACTION COMPLETED: 3-M CAPABILITY UTILIZED</b> 0. NONE OF THE ABOVE, DESCRIBE IN REMARKS/DESCRIPTION SECTION		1. MANDATORY 2. ESSENTIAL 3. HIGHLY DESIRABLE 4. <b>DESIRABLE</b>			
DISCREPANCY DESCRIPTION: PER SEMAT I INSPECTION							
STBD side (5-292-0-E), Potable water tank (5-292-1-W), top of tank has heavy pitting, corrosion and paint separation. Several low U/T readings of <b>.112, .152, .181 and .190</b> . Approximately 60sf of tank top. Design thickness is .250 and the minimum is .213.							
RECOMMEND REPAIR ACTIVITY: Cut out and replace approximately 60sf of tank top with .250 steel. Preserve and paint to match area. FTSC is to conduct risk assessment and to make final repair recommendations.							
SSPORT C221 POC: F. PRAISSMAN (757) 396-4001 EXT 2130, TPOC: T. ZWOLAK							
CSMP SUMMARY: Potable water tank top has heavy deterioration.					TEST# MRC 1102/1		
ROOT CAUSE/AMPLIFICATION: H1					STEP# 1,2,3,4 & 5		
SYSTEM LEVEL IMPACT:							
PART NO.	NOMEMCLATURE			QTY	NSN	COST	
1							
2							
3							
4							
5							
6							
FIRST CONTACT LARSEN	RATE EN3	SECOND CONTACT LARSEN		RATE EN3	PHONE 22		
ASSESSOR / ACT / TECH ID# J. LaROCK / T. ILGENFRITZ QED SYSTEMS				TD	TL	LOGISTICIAN	TSP

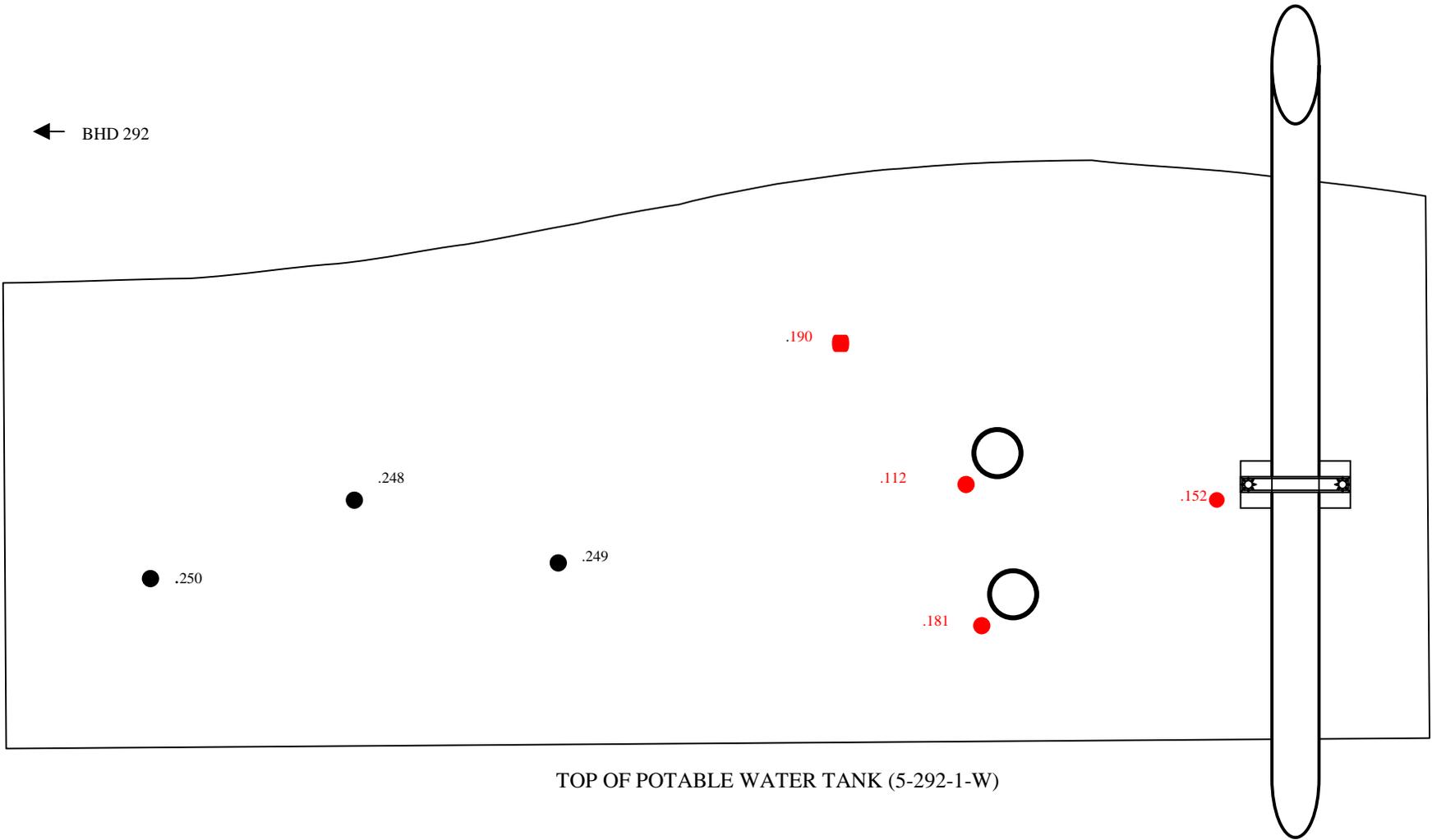
**AUXILIARY MACHINERY ROOM 3 (5-292-0-E)**



STBD SIDE, POTABLE WATER TANK (5-292-1-W)  
U/T READINGS NOTED IN DRAWING.



STBD SIDE, POTABLE WATER TANK (5-292-1-W)  
PIPE CLAMP ON TOP OF TANK. U/T READINGS ARE NOTED ON DRAWING.



USS SIMPSON (FFG 56)  
 AUXILIARY MACHINERY ROOM #3  
 (5-292-0-E) STBD SIDE

J. LaROCK / T. ILGENFRITZ  
 QED SYSTEMS INC  
 06/08/01

# MATERIAL ASSESSMENT FORM

ITEM NUMBER				SHIP <b>USS SIMPSON FFG 56</b>				
SYSTEM Hull Structure		ESWBS 11011	EIC A100	APL X-SYSTEMS		RIN N/L		
EQUIPMENT NAME Shell & Supporting Structure		IDENT/SERIAL N/A		LOCATION 5-292-0-E	WORK CENTER EMO2	JSN P043		
EQUIPMENT STATUS		PROBLEM STATUS		CATEGORY			EOC	
1. SAT - SATISFACTORY 2. <b>UNSAT - UNSATISFACTORY</b> 3. TNA - TEST NOT ATTEMPTED 4. TNC - TEST NOT COMPLETED 5. NOB - EQUIPMENT NOT ONBOARD		1. C - CORRECTED 2. A - AWAITING PARTS 3. T - AWAITING TECH ASSIST* 4. <b>S - SHIP'S FORCE</b> *UNCORRECTED PROBLEMS		1. S - PERSONNEL SAFETY 2. Z - EQUIPMENT SAFETY 3. <b>H - HARDWARE</b> 4. D - DOWN 5. L - LIMITED			<b>.9</b>	
TYPE/AVAILABILITY	WHEN TO BE ACCOMPLISHED	STATUS	CAUSE		DEFERRAL REASON			
1. DEPOT 2. IMA 3. TECH ASSIST 4. <b>SHIP'S FORCE</b> 5. SFWL	1. IMMEDIATELY 2. PRIOR TO NEXT UNDERWAY 3. PRIOR TO NEXT DEPLOYMENT 4. <b>DURING POST DEPLOYMENT AVAILABILITY</b>	1. <b>OPERATIONAL</b> 2. NON-OPERATIONAL 3. REDUCED CAPABILITY 0. NOT APPLICABLE	1. ABNORMAL ENVIRONMENT 2. MANUFACTURER/INSTALLATION DEFECTS 3. LACK OF KNOWLEDGE 4. COMMUNICATION PROBLEMS 5. INADEQUATE INSTRUCTION/PROCEDURE 6. INADEQUATE DESIGN 7. <b>NORMAL WEAR AND TEAR</b> 0. NOT APPLICABLE		1. <b>S/F BACKLOG/OPERATIONAL PRIORITY</b> 2. LACK OF MATERIAL 3. NO FORMAL TRAINING ON THIS EQUIPMENT 4. FORMAL TRAINING INADEQUATE IN THIS EQUIPMENT 5. INADEQUATE SCHOOL PRACTICAL TRAINING 6. LACK OF FACILITIES/CAPABILITIES 7. NOT AUTHORIZED FOR S/F ACCOMPLISHMENT 8. FOR S/F OVERHAUL OR AVAILABILITY WORK LIST 9. LACK OF TECHNICAL DOCUMENTATION 0. OTHER OR NOT APPLICABLE			
MAN HRS EXPENDED 2	MAN HRS REMAINING 100	COMPLETED ACTION TAKEN		S/F MANHOURS	PRIORITY			
<b>SAFETY HAZARD:</b> 1. SERIOUS - CORRECT AS SOON AS POSSIBLE 2. SERIOUS - SUSPENSION OF EQUIPMENT/SYSTEM/SPACE REQUIRED 3. SERIOUS - WAIVER OF EQUIPMENT/SYSTEM 4. <b>SAFETY ITEM - MINOR</b> 5. COMBUSTIBLE MATERIAL		1. MAINTENANCE ACTION COMPLETED: PARTS DRAWN FROM SUPPLY 2. MAINTENANCE ACTION COMPLETED: REQUIRED PARTS NOT DRAWN FROM SUPPLY 3. MAINTENANCE ACTION COMPLETED: NO PARTS REQUIRED 4. CANCELLED - DEFERRAL WILL BE REMOVED FROM CSMP 7. <b>MAINTENANCE ACTION COMPLETED: 3-M CAPABILITY UTILIZED</b> 0. NONE OF THE ABOVE, DESCRIBE IN REMARKS/DESCRIPTION SECTION			1. MANDATORY 2. ESSENTIAL 3. HIGHLY DESIRABLE 4. <b>DESIRABLE</b>			
DISCREPANCY DESCRIPTION: PER SEMAT I INSPECTION STBD side (5-292-0-E), bilge pocket between WF(300-308), has light corrosion and paint separation. Approximately 24sf.								
RECOMMENDED REPAIRS: Needle-gun approximately 24sf of bilge pocket, preserve and paint to match.								
SSPORT C221 POC: F. PRAISSMAN (757) 396-4001 EXT 2130, TPOC: T. ZWOLAK								
CSMP SUMMARY: Bilge pocket has corrosion and paint separation.					TEST# MRC 1102/1			
ROOT CAUSE/AMPLIFICATION: H1					STEP# 1,2,3,4 & 5			
SYSTEM LEVEL IMPACT:								
PART NO.	NOMEMCLATURE			QTY	NSN		COST	
1								
2								
3								
4								
5								
6								
FIRST CONTACT LARSEN	RATE EN3	SECOND CONTACT LARSEN		RATE EN3	PHONE 22			
ASSESSOR / ACT / TECH ID# J. LaROCK / T. ILGENFRITZ QED SYSTEMS				TD	TL	LOGISTICIAN	TSP	

**AUXILIARY MACHINERY ROOM 3 (5-292-0-E)**



STBD SIDE. BILGE POCKET; BETWEEN WEB FRAME (300-308).  
AREA RECOMMENDED FOR PRESERVATION.

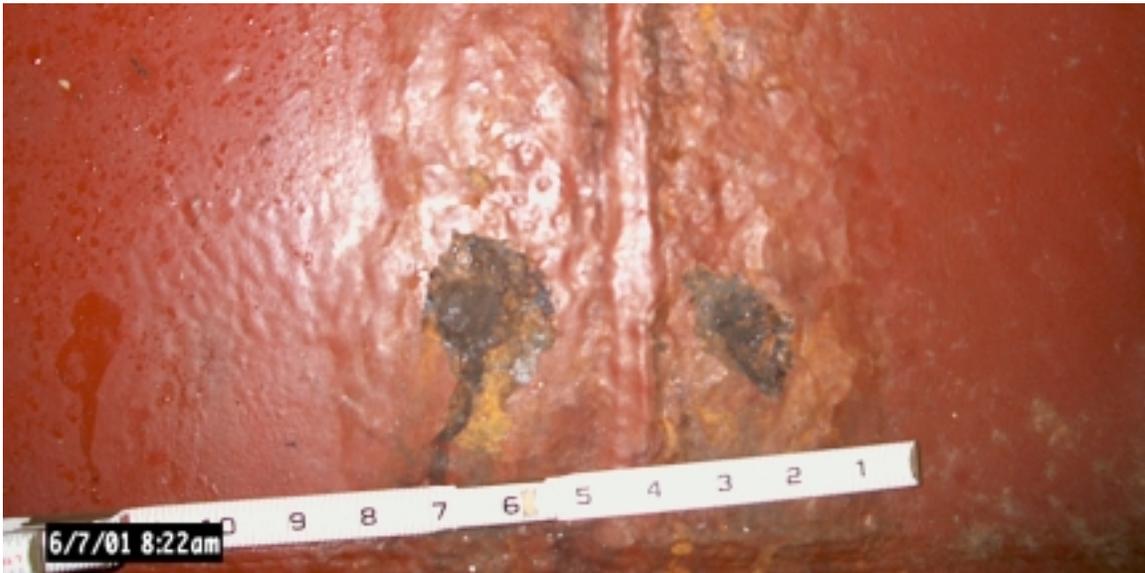


STBE SIDE BETWEEN STRINGERS (8-9) 2FT FWD OF WEB FRAME 300  
AREA RECOMMENDED FOR PRESERVATION.

# MATERIAL ASSESSMENT FORM

ITEM NUMBER				SHIP <b>USS SIMPSON FFG 56</b>				
SYSTEM Hull Structure		ESWBS 11011	EIC A100	APL X-SYSTEMS		RIN N/L		
EQUIPMENT NAME Shell & Supporting Structure		IDENT/SERIAL N/A		LOCATION 5-250-0-E	WORK CENTER EMO2	JSN P047		
EQUIPMENT STATUS		PROBLEM STATUS		CATEGORY			EOC	
1. SAT - SATISFACTORY 2. <b>UNSAT - UNSATISFACTORY</b> 3. TNA - TEST NOT ATTEMPTED 4. TNC - TEST NOT COMPLETED 5. NOB - EQUIPMENT NOT ONBOARD		1. C - CORRECTED 2. A - AWAITING PARTS 3. <b>T - AWAITING TECH ASSIST*</b> 4. S - SHIP'S FORCE *UNCORRECTED PROBLEMS		1. S - PERSONNEL SAFETY 2. Z - EQUIPMENT SAFETY 3. <b>H - HARDWARE</b> 4. D - DOWN 5. L - LIMITED			<b>.9</b>	
TYPE/AVAILABILITY	WHEN TO BE ACCOMPLISHED	STATUS		CAUSE		DEFERRAL REASON		
1. DEPOT 2. IMA 3. <b>TECH ASSIST</b> 4. SHIP'S FORCE 5. SFWL	1. IMMEDIATELY 2. PRIOR TO NEXT UNDERWAY 3. PRIOR TO NEXT DEPLOYMENT 4. <b>DURING POST DEPLOYMENT AVAILABILITY</b>	1. <b>OPERATIONAL</b> 2. NON-OPERATIONAL 3. REDUCED CAPABILITY 0. NOT APPLICABLE		1. ABNORMAL ENVIRONMENT 2. MANUFACTURER/INSTALLATION DEFECTS 3. LACK OF KNOWLEDGE 4. COMMUNICATION PROBLEMS 5. INADEQUATE INSTRUCTION/PROCEDURE 6. INADEQUATE DESIGN 7. <b>NORMAL WEAR AND TEAR</b> 0. NOT APPLICABLE		1. S/F BACKLOG/OPERATIONAL PRIORITY 2. LACK OF MATERIAL 3. NO FORMAL TRAINING ON THIS EQUIPMENT 4. FORMAL TRAINING INADEQUATE IN THIS EQUIPMENT 5. INADEQUATE SCHOOL PRACTICAL TRAINING 6. LACK OF FACILITIES/CAPABILITIES 7. <b>NOT AUTHORIZED FOR S/F ACCOMPLISHMENT</b> 8. FOR S/F OVERHAUL OR AVAILABILITY WORK LIST 9. LACK OF TECHNICAL DOCUMENTATION 0. OTHER OR NOT APPLICABLE		
MAN HRS EXPENDED 2	MAN HRS REMAINING 600		COMPLETED ACTION TAKEN		S/F MANHOURS	PRIORITY		
<b>SAFETY HAZARD:</b> 1. SERIOUS - CORRECT AS SOON AS POSSIBLE 2. SERIOUS - SUSPENSION OF EQUIPMENT/SYSTEM/SPACE REQUIRED 3. SERIOUS - WAIVER OF EQUIPMENT/SYSTEM 4. <b>SAFETY ITEM - MINOR</b> 5. COMBUSTIBLE MATERIAL			1. MAINTENANCE ACTION COMPLETED: PARTS DRAWN FROM SUPPLY 2. MAINTENANCE ACTION COMPLETED: REQUIRED PARTS NOT DRAWN FROM SUPPLY 3. MAINTENANCE ACTION COMPLETED: NO PARTS REQUIRED 4. CANCELLED - DEFERRAL WILL BE REMOVED FROM CSMP 7. <b>MAINTENANCE ACTION COMPLETED: 3-M CAPABILITY UTILIZED</b> 0. NONE OF THE ABOVE, DESCRIBE IN REMARKS/DESCRIPTION SECTION			1. MANDATORY 2. ESSENTIAL 3. HIGHLY DESIRABLE 4. <b>DESIRABLE</b>		
DISCREPANCY DESCRIPTION: PER SEMAT I INSPECTION <b>Port side (5-250-0-E), between WF (264-270), shell plate from stringers (11-13) is thinning with low U/T readings at .240, .248, .296 and .301. The design thickness is .313, the readings are within the 25% wastage, Transverse weld seam AFT of WF (270), from stringers (11-12), has shell thinned at .273, AFT of weld seam and .276 FWD of weld seam. Aft of transverse weld seam where "B" strake meets "C" strake, shell is thinning at .426, which is below the 25% wastage of .500 plate.</b>								
RECOMMEND REPAIR ACTIVITY: Cut out and replace approximately 30sf of shell plate from stringer (11-13), with .313 steel. Approximately 4sf of shell plate between stringers (7-8), at transverse weld seam, replace with .500 steel. Preserve and paint to match area. <b>FTSC IS TO CONDUCT RISK ASSESSMENT AND MAKE FINAL REPAIR RECOMMENDATIONS.</b>								
SSPORT C221 POC: F. PRAISSMAN (757) 396-4001 EXT 2130, TPOC: T. ZWOLAK								
CSMP SUMMARY: Shell plate is thinning.					TEST# MRC 1102/1			
ROOT CAUSE/AMPLIFICATION: H1					STEP# 1,2,3,4 & 5			
SYSTEM LEVEL IMPACT:								
PART NO.	NOMEMCLATURE			QTY	NSN		COST	
1								
2								
3								
4								
5								
6								
FIRST CONTACT LARSEN	RATE EN3	SECOND CONTACT LARSEN		RATE EN3	PHONE 22			
ASSESSOR / ACT / TECH ID# J. LaROCK / T. ILGENFRITZ QED SYSTEMS				TD	TL	LOGISTICIAN	TSP	

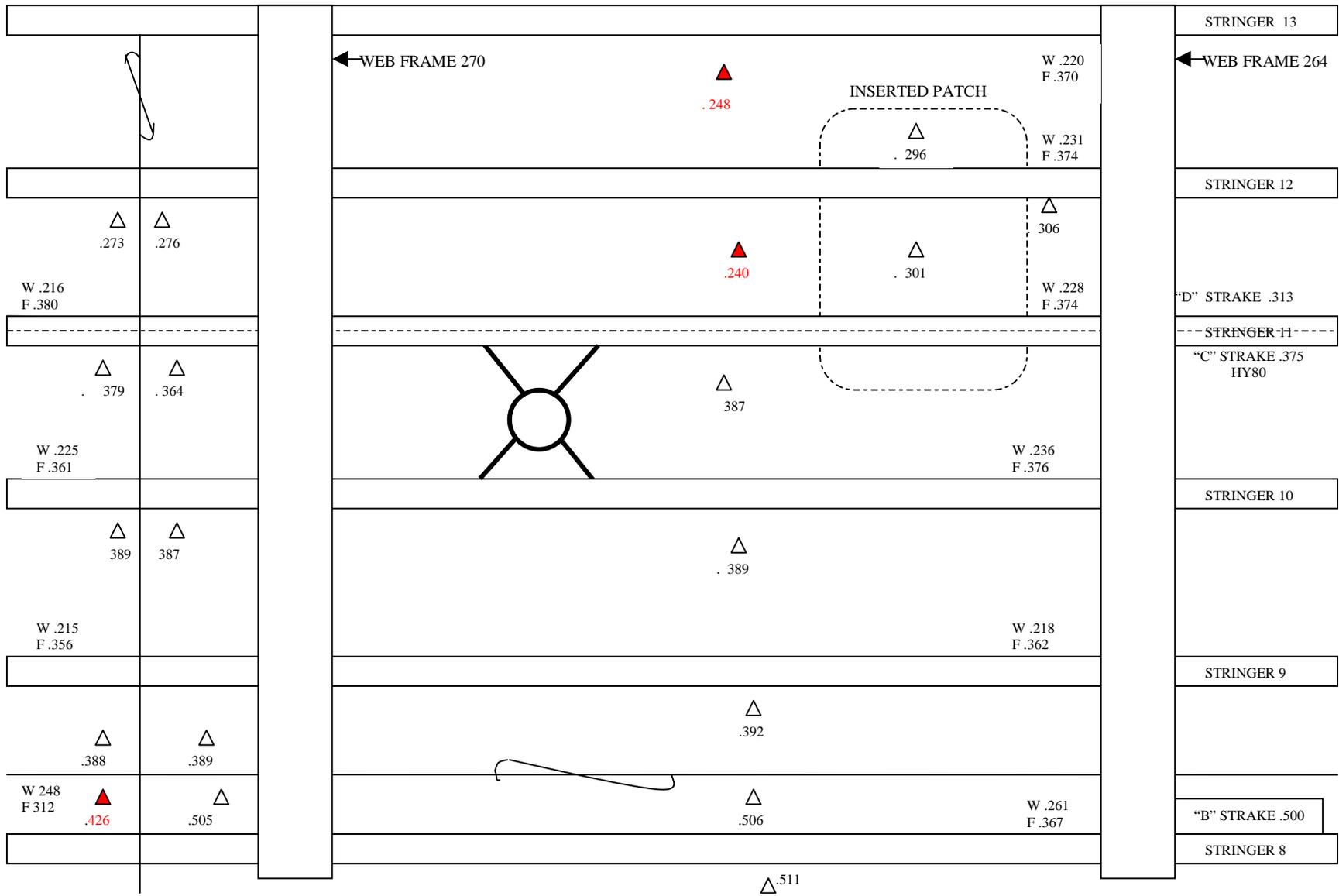
**MAIN ENGINE ROOM (5-250-0-E)**



PORT SIDE. TRANSVERSE WELD SEAM, AFT OF WEB FRAME 270.  
HISTORICAL HOT SPOT, SHELL IS THINNING AND U/T READINGS ARE  
INDICATED IN DRAWING.



PORT SIDE ABOVE STRINGER 12  
AREA RECOMMENDED FOR PRESERVATION.



△	.SHELL PLATE
W	STRINGER WEB
F	STRINGER FLANGE

USS SIMPSON (FFG 56)  
 AUXILIARY ROOM #3  
 (5-292-0-E) PORT SIDE

J. LaROCK / T. ILGENFRITZ  
 QED SYSTEMS INC  
 06/08/01

# MATERIAL ASSESSMENT FORM

ITEM NUMBER				SHIP <b>USS SIMPSON FFG 56</b>				
SYSTEM Hull Structure		ESWBS 11011	EIC A100	APL X-SYSTEMS		RIN N/L		
EQUIPMENT NAME Shell & Supporting Structure		IDENT/SERIAL N/A		LOCATION 5-250-0-E	WORK CENTER EMO2	JSN P041		
EQUIPMENT STATUS		PROBLEM STATUS		CATEGORY			EOC	
1. SAT - SATISFACTORY 2. <b>UNSAT - UNSATISFACTORY</b> 3. TNA - TEST NOT ATTEMPTED 4. TNC - TEST NOT COMPLETED 5. NOB - EQUIPMENT NOT ONBOARD		1. C - CORRECTED 2. A - AWAITING PARTS 3. T - AWAITING TECH ASSIST* 4. <b>S - SHIP'S FORCE</b> *UNCORRECTED PROBLEMS		1. S - PERSONNEL SAFETY 2. Z - EQUIPMENT SAFETY 3. <b>H - HARDWARE</b> 4. D - DOWN 5. L - LIMITED			<b>.9</b>	
TYPE/AVAILABILITY	WHEN TO BE ACCOMPLISHED	STATUS		CAUSE		DEFERRAL REASON		
1. DEPOT 2. IMA 3. TECH ASSIST 4. <b>SHIP'S FORCE</b> 5. SFWL	1. IMMEDIATELY 2. PRIOR TO NEXT UNDERWAY 3. PRIOR TO NEXT DEPLOYMENT 4. <b>DURING POST DEPLOYMENT AVAILABILITY</b>	1. <b>OPERATIONAL</b> 2. NON-OPERATIONAL 3. REDUCED CAPABILITY 0. NOT APPLICABLE		1. ABNORMAL ENVIRONMENT 2. MANUFACTURER/INSTALLATION DEFECTS 3. LACK OF KNOWLEDGE 4. COMMUNICATION PROBLEMS 5. INADEQUATE INSTRUCTION/PROCEDURE 6. INADEQUATE DESIGN 7. <b>NORMAL WEAR AND TEAR</b> 0. NOT APPLICABLE		1. <b>S/F BACKLOG/OPERATIONAL PRIORITY</b> 2. LACK OF MATERIAL 3. NO FORMAL TRAINING ON THIS EQUIPMENT 4. FORMAL TRAINING INADEQUATE IN THIS EQUIPMENT 5. INADEQUATE SCHOOL PRACTICAL TRAINING 6. LACK OF FACILITIES/CAPABILITIES 7. NOT AUTHORIZED FOR S/F ACCOMPLISHMENT 8. FOR S/F OVERHAUL OR AVAILABILITY WORK LIST 9. LACK OF TECHNICAL DOCUMENTATION 0. OTHER OR NOT APPLICABLE		
MAN HRS EXPENDED 2	MAN HRS REMAINING 100	COMPLETED ACTION TAKEN		S/F MANHOURS	PRIORITY			
<b>SAFETY HAZARD:</b> 1. SERIOUS - CORRECT AS SOON AS POSSIBLE 2. SERIOUS - SUSPENSION OF EQUIPMENT/SYSTEM/SPACE REQUIRED 3. SERIOUS - WAIVER OF EQUIPMENT/SYSTEM 4. <b>SAFETY ITEM - MINOR</b> 5. COMBUSTIBLE MATERIAL		1. MAINTENANCE ACTION COMPLETED: PARTS DRAWN FROM SUPPLY 2. MAINTENANCE ACTION COMPLETED: REQUIRED PARTS NOT DRAWN FROM SUPPLY 3. MAINTENANCE ACTION COMPLETED: NO PARTS REQUIRED 4. CANCELLED - DEFERRAL WILL BE REMOVED FROM CSMP 7. <b>MAINTENANCE ACTION COMPLETED: 3-M CAPABILITY UTILIZED</b> 0. NONE OF THE ABOVE, DESCRIBE IN REMARKS/DESCRIPTION SECTION			1. MANDATORY 2. ESSENTIAL 3. HIGHLY DESIRABLE 4. <b>DESIRABLE</b>			
DISCREPANCY DESCRIPTION: PER SEMAT I INSPECTION								
Port side (5-250-0-E), Between WF (250-264) between stringers (10-11), approximately 40sf of saturated lagging, with light paint								
Separation. U/T reading under the Eductor overboard (3-255-2) shell is thinning at .297. but is within tolerance of design								
thickness of .313.								
RECOMMENDED REPAIRS:								
Remove approximately 40sf of saturated lagging, needle-gun and preserve areas exposed, replace with 2" hull board and paint to match.								
SSPORT C221 POC: F. PRAISSMAN (757) 396-4001 EXT 2130, TPOC: T. ZWOLAK								
CSMP SUMMARY: Lagging is saturated, light corrosion under lagging.						TEST# MRC 1102/1		
ROOT CAUSE/AMPLIFICATION: H1						STEP# 1,2,3,4 & 5		
SYSTEM LEVEL IMPACT:								
PART NO.	NOMEMCLATURE			QTY	NSN		COST	
1								
2								
3								
4								
5								
6								
FIRST CONTACT LARSEN	RATE EN3	SECOND CONTACT LARSEN		RATE EN3	PHONE 22			
ASSESSOR / ACT / TECH ID# J. LaROCK / T. ILGENFRITZ QED SYSTEMS				TD	TL	LOGISTICIAN	TSP	

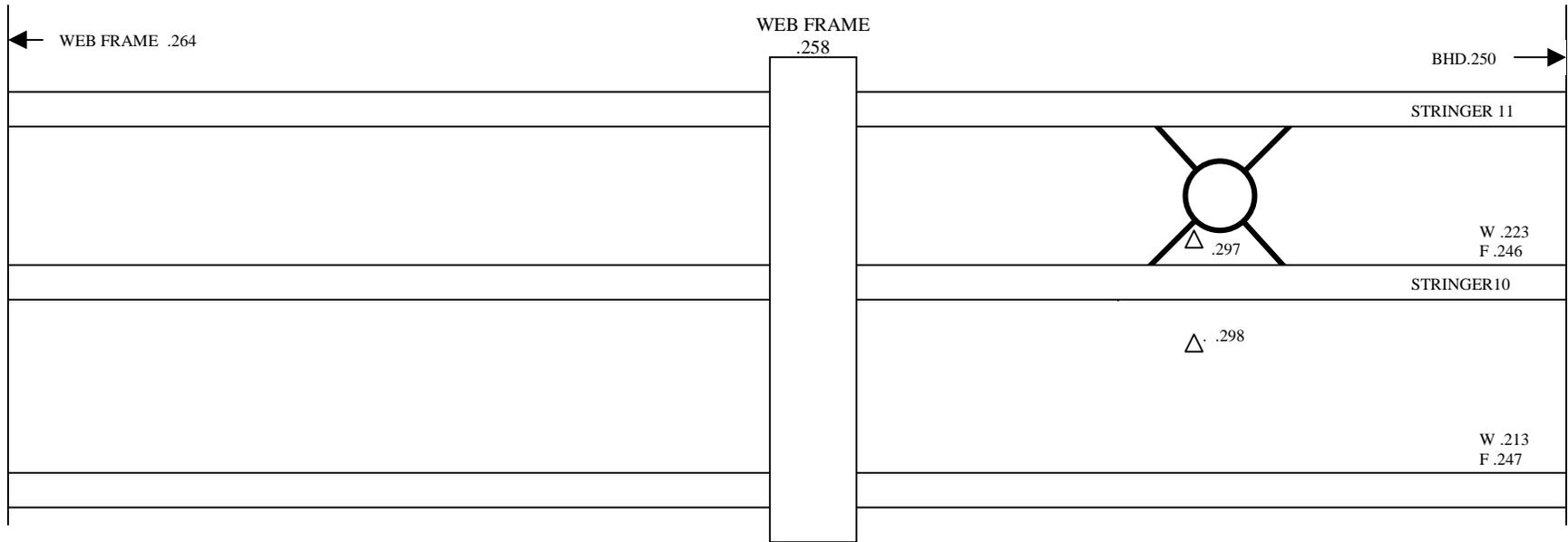
**MAIN ENGINE ROOM (5-250-0-E)**



PORT SIDE, AFT OF OVBD EDUCTOR AT STRINGER 10



PORT SIDE. BETWEEN WEB FRAMES (250-258)  
UNDER OVER BOARD EDUCTOR, LAGGING IS SATURATED.  
REPLACEMENT RECOMMENDED. U/T READINGS ARE NOTED IN DRAWING.



Δ	. SHELL PLATE
W	STRINGER WEB
F	STRINGER FLANGE

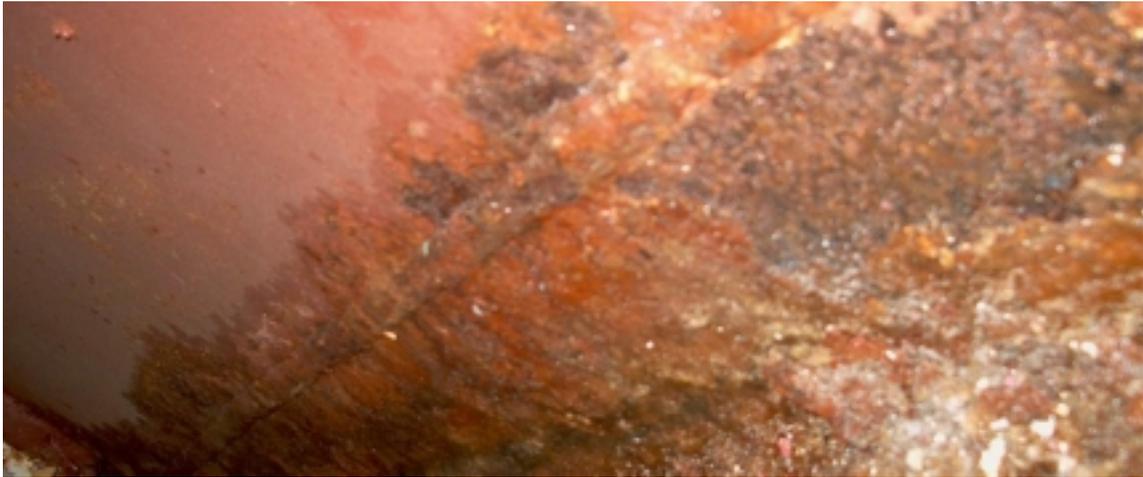
USS SIMPSON (FFG 56)  
 MAIN ENGINE ROOM  
 (5-250-0-E) PORT SIDE

J. LaROCK / T. ILGENFRITZ  
 QED SYSTEMS INC  
 06/08/01

# MATERIAL ASSESSMENT FORM

ITEM NUMBER				SHIP <b>USS SIMPSON FFG 56</b>			
SYSTEM Hull Structure		ESWBS 11011	EIC A100	APL X-SYSTEMS		RIN N/L	
EQUIPMENT NAME Shell & Supporting Structure		IDENT/SERIAL N/A		LOCATION 5-212-0-E	WORK CENTER EMO2	JSN P044	
EQUIPMENT STATUS		PROBLEM STATUS		CATEGORY			EOC
1. SAT - SATISFACTORY 2. <b>UNSAT - UNSATISFACTORY</b> 3. TNA - TEST NOT ATTEMPTED 4. TNC - TEST NOT COMPLETED 5. NOB - EQUIPMENT NOT ONBOARD		1. C - CORRECTED 2. A - AWAITING PARTS 3. T - AWAITING TECH ASSIST* 4. <b>S - SHIP'S FORCE</b> *UNCORRECTED PROBLEMS		1. S - PERSONNEL SAFETY 2. Z - EQUIPMENT SAFETY 3. <b>H - HARDWARE</b>		4. D - DOWN 5. L - LIMITED	
<b>TYPE/AVAILABILITY</b> 1. DEPOT 2. IMA 3. TECH ASSIST 4. <b>SHIP'S FORCE</b> 5. SFWL		<b>WHEN TO BE ACCOMPLISHED</b> 1. IMMEDIATELY 2. PRIOR TO NEXT UNDERWAY 3. PRIOR TO NEXT DEPLOYMENT 4. <b>DURING POST DEPLOYMENT AVAILABILITY</b>		<b>STATUS</b> 1. <b>OPERATIONAL</b> 2. NON-OPERATIONAL 3. REDUCED CAPABILITY 0. NOT APPLICABLE		<b>CAUSE</b> 1. ABNORMAL ENVIRONMENT 2. MANUFACTURER/INSTALLATION DEFECTS 3. LACK OF KNOWLEDGE 4. COMMUNICATION PROBLEMS 5. INADEQUATE INSTRUCTION/PROCEDURE 6. INADEQUATE DESIGN 7. <b>NORMAL WEAR AND TEAR</b> 0. NOT APPLICABLE	
				<b>DEFERRAL REASON</b> 1. <b>S/F BACKLOG/OPERATIONAL PRIORITY</b> 2. LACK OF MATERIAL 3. NO FORMAL TRAINING ON THIS EQUIPMENT 4. FORMAL TRAINING INADEQUATE IN THIS EQUIPMENT 5. INADEQUATE SCHOOL PRACTICAL TRAINING 6. LACK OF FACILITIES/CAPABILITIES 7. NOT AUTHORIZED FOR S/F ACCOMPLISHMENT 8. FOR S/F OVERHAUL OR AVAILABILITY WORK LIST 9. LACK OF TECHNICAL DOCUMENTATION 0. OTHER OR NOT APPLICABLE		<b>.9</b>	
MAN HRS EXPENDED 2		MAN HRS REMAINING 100		COMPLETED ACTION TAKEN		S/F MANHOURS	
<b>SAFETY HAZARD:</b> 1. SERIOUS - CORRECT AS SOON AS POSSIBLE 2. SERIOUS - SUSPENSION OF EQUIPMENT/SYSTEM/SPACE REQUIRED 3. SERIOUS - WAIVER OF EQUIPMENT/SYSTEM 4. <b>SAFETY ITEM - MINOR</b> 5. COMBUSTIBLE MATERIAL		1. MAINTENANCE ACTION COMPLETED: PARTS DRAWN FROM SUPPLY 2. MAINTENANCE ACTION COMPLETED: REQUIRED PARTS NOT DRAWN FROM SUPPLY 3. MAINTENANCE ACTION COMPLETED: NO PARTS REQUIRED 4. CANCELLED - DEFERRAL WILL BE REMOVED FROM CSMP 7. <b>MAINTENANCE ACTION COMPLETED: 3-M CAPABILITY UTILIZED</b> 0. NONE OF THE ABOVE, DESCRIBE IN REMARKS/DESCRIPTION SECTION				1. MANDATORY 2. ESSENTIAL 3. HIGHLY DESIRABLE 4. <b>DESIRABLE</b>	
DISCREPANCY DESCRIPTION: PER SEMAT I INSPECTION							
Port side (5-212-0-E), from WF (220-228) between stringers (7-11), approximately 144sf of surface rust and paint separation. U/T readings were Satisfactory with minor reduction in design thickness.							
RECOMMENDED REPAIRS: Needle-gun approximately 144sf of area, preserve and paint to match.							
SSPORT C221 POC: F. PRAISSMAN (757) 396-4001 EXT 2130, TPOC: T. ZWOLAK							
CSMP SUMMARY: Area specified has light corrosion and paint separation.						TEST# MRC 1102/1	
ROOT CAUSE/AMPLIFICATION: H1						STEP# 1,2,3,4 & 5	
SYSTEM LEVEL IMPACT:							
PART NO.		NOMEMCLATURE		QTY	NSN	COST	
1							
2							
3							
4							
5							
6							
FIRST CONTACT LARSEN		RATE EN3	SECOND CONTACT LARSEN		RATE EN3	PHONE 22	
ASSESSOR / ACT / TECH ID# J. LaROCK / T. ILGENFRITZ QED SYSTEMS				TD	TL	LOGISTICIAN	TSP

**AUXILIARY MACHINERY ROOM 2 (5-212-0-E)**



PORT SIDE, AFT OF WEB FRAME 220, WELD SEAM WHERE "C" AND "D" STRAKE MEET.  
U/T READINGS ARE NOTED ON DRAWING.



PORT SIDE, BETWEEN STRINGERS (8-9) AFT OF WEB FRAME 220.  
PRESERVATION AND PAINT RECOMMENDED.

W .215  
F .360

△ 334

WEB FRAME  
220

△ . 316

W .222  
F .372

STRINGER 11

← WEB FRAME 228

△ .328

△ .313

BHD 212 →

"D" STRAKE .313

W .224  
F .395

△ . 375

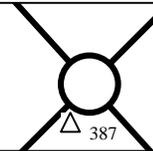
△ 378

W .215  
F .364

STRINGER 10

W .214  
F .360

△ .379



W .245  
F .356

STRINGER 9

W .215  
F .361

△ .390

△ . 384

"C" STRAKE .375  
HY80

W .219  
F .380

STRINGER 8

W .257  
F .380

△ 382

△ . 386

W .258  
F .384

STRINGER 7

△ .384

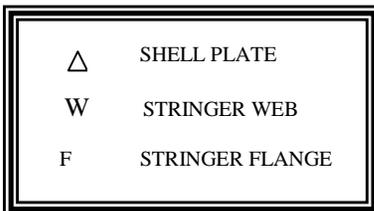
△ .384

"B" STRAKE .500

△ .518

STRINGER 6

32



USS SIMPSON (FFG 56)  
AUX ILARY ROOM #3  
(5-292-0-E) PORT SIDE

J. LaROCK / T. ILGENFRITZ  
QED SYSTEMS INC  
06/08/01

# MATERIAL ASSESSMENT FORM

ITEM NUMBER				SHIP <b>USS SIMPSON FFG 56</b>			
SYSTEM Hull Structure		ESWBS 11011	EIC A100	APL X-SYSTEMS		RIN N/L	
EQUIPMENT NAME Shell & Supporting Structure		IDENT/SERIAL N/A		LOCATION 5-212-0-E	WORK CENTER EMO2	JSN P045	
EQUIPMENT STATUS		PROBLEM STATUS		CATEGORY			EOC
1. SAT - SATISFACTORY 2. <b>UNSAT - UNSATISFACTORY</b> 3. TNA - TEST NOT ATTEMPTED 4. TNC - TEST NOT COMPLETED 5. NOB - EQUIPMENT NOT ONBOARD		1. C - CORRECTED 2. A - AWAITING PARTS 3. T - AWAITING TECH ASSIST* 4. <b>S - SHIP'S FORCE</b> *UNCORRECTED PROBLEMS		1. S - PERSONNEL SAFETY 2. Z - EQUIPMENT SAFETY 3. <b>H - HARDWARE</b>		4. D - DOWN 5. L - LIMITED  <b>.9</b>	
TYPE/AVAILABILITY	WHEN TO BE ACCOMPLISHED	STATUS	CAUSE		DEFERRAL REASON		
1. DEPOT 2. IMA 3. TECH ASSIST 4. <b>SHIP'S FORCE</b> 5. SFWL	1. IMMEDIATELY 2. PRIOR TO NEXT UNDERWAY 3. PRIOR TO NEXT DEPLOYMENT 4. <b>DURING POST DEPLOYMENT AVAILABILITY</b>	1. <b>OPERATIONAL</b> 2. NON-OPERATIONAL 3. REDUCED CAPABILITY 0. NOT APPLICABLE	1. ABNORMAL ENVIRONMENT 2. MANUFACTURER/INSTALLATION DEFECTS 3. LACK OF KNOWLEDGE 4. COMMUNICATION PROBLEMS 5. INADEQUATE INSTRUCTION/PROCEDURE 6. INADEQUATE DESIGN 7. <b>NORMAL WEAR AND TEAR</b> 0. NOT APPLICABLE		1. <b>S/F BACKLOG/OPERATIONAL PRIORITY</b> 2. LACK OF MATERIAL 3. NO FORMAL TRAINING ON THIS EQUIPMENT 4. FORMAL TRAINING INADEQUATE IN THIS EQUIPMENT 5. INADEQUATE SCHOOL PRACTICAL TRAINING 6. LACK OF FACILITIES/CAPABILITIES 7. NOT AUTHORIZED FOR S/F ACCOMPLISHMENT 8. FOR S/F OVERHAUL OR AVAILABILITY WORK LIST 9. LACK OF TECHNICAL DOCUMENTATION 0. OTHER OR NOT APPLICABLE		
MAN HRS EXPENDED 2	MAN HRS REMAINING 100	COMPLETED ACTION TAKEN		S/F MANHOURS	PRIORITY		
<b>SAFETY HAZARD:</b> 1. SERIOUS - CORRECT AS SOON AS POSSIBLE 2. SERIOUS - SUSPENSION OF EQUIPMENT/SYSTEM/SPACE REQUIRED 3. SERIOUS - WAIVER OF EQUIPMENT/SYSTEM 4. <b>SAFETY ITEM - MINOR</b> 5. COMBUSTIBLE MATERIAL		1. MAINTENANCE ACTION COMPLETED: PARTS DRAWN FROM SUPPLY 2. MAINTENANCE ACTION COMPLETED: REQUIRED PARTS NOT DRAWN FROM SUPPLY 3. MAINTENANCE ACTION COMPLETED: NO PARTS REQUIRED 4. CANCELLED - DEFERRAL WILL BE REMOVED FROM CSMP 7. <b>MAINTENANCE ACTION COMPLETED: 3-M CAPABILITY UTILIZED</b> 0. NONE OF THE ABOVE, DESCRIBE IN REMARKS/DESCRIPTION SECTION			1. MANDATORY 2. ESSENTIAL 3. HIGHLY DESIRABLE 4. <b>DESIRABLE</b>		
DISCREPANCY DESCRIPTION: PER SEMAT I INSPECTION							
Port side (5-212-0-E), from Frame (328-236) between stringers (3-5), has approximately 50sf of surface rust and paint Separation.							
RECOMMENDED REPAIRS: Needle-gun approximately 50sf of area, preserve and paint to match.							
SSPORT C221 POC: F. PRAISSMAN (757) 396-4001 EXT 2130, TPOC: T. ZWOLAK							
CSMP SUMMARY: Area specified has light corrosion and paint separation.					TEST# MRC 1102/1		
ROOT CAUSE/AMPLIFICATION: H1					STEP# 1,2,3,4 & 5		
SYSTEM LEVEL IMPACT:							
PART NO.	NOMEMCLATURE			QTY	NSN	COST	
1							
2							
3							
4							
5							
6							
FIRST CONTACT LARSEN	RATE EN3	SECOND CONTACT LARSEN		RATE EN3	PHONE 22		
ASSESSOR / ACT / TECH ID# J. LaROCK / T. ILGENFRITZ QED SYSTEMS				TD	TL	LOGISTICIAN	TSP

**AUXILIARY MACHINERY ROOM 2 (5-212-0-E)**



PORT SIDE, AFT OF WEB FRAME 220, BILGE AREA RRECOMMENDED FOR PRESERVATION AND PAINT.



PORT SIDE, FORWARD OP WEB FRAME 236,BILGE AREA PRESERVATION AND PAINT RECOMMENDED.

# MATERIAL ASSESSMENT FORM

ITEM NUMBER				SHIP <b>USS SIMPSON FFG 56</b>				
SYSTEM Hull Structure		ESWBS 11011	EIC A100	APL X-SYSTEMS		RIN N/L		
EQUIPMENT NAME Shell & Supporting Structure		IDENT/SERIAL N/A		LOCATION 4-160-0-Q	WORK CENTER EMO4	JSN P452		
EQUIPMENT STATUS		PROBLEM STATUS		CATEGORY			EOC	
1. SAT - SATISFACTORY 2. <b>UNSAT - UNSATISFACTORY</b> 3. TNA - TEST NOT ATTEMPTED 4. TNC - TEST NOT COMPLETED 5. NOB - EQUIPMENT NOT ONBOARD		1. C - CORRECTED 2. A - AWAITING PARTS 3. T - AWAITING TECH ASSIST* 4. <b>S - SHIP'S FORCE</b> *UNCORRECTED PROBLEMS		1. S - PERSONNEL SAFETY 2. Z - EQUIPMENT SAFETY 3. <b>H - HARDWARE</b> 4. D - DOWN 5. L - LIMITED			<b>.9</b>	
TYPE/AVAILABILITY	WHEN TO BE ACCOMPLISHED	STATUS		CAUSE		DEFERRAL REASON		
1. DEPOT 2. IMA 3. TECH ASSIST 4. <b>SHIP'S FORCE</b> 5. SFWL	1. IMMEDIATELY 2. PRIOR TO NEXT UNDERWAY 3. PRIOR TO NEXT DEPLOYMENT 4. <b>DURING POST DEPLOYMENT AVAILABILITY</b>	1. <b>OPERATIONAL</b> 2. NON-OPERATIONAL 3. REDUCED CAPABILITY 0. NOT APPLICABLE		1. ABNORMAL ENVIRONMENT 2. MANUFACTURER/INSTALLATION DEFECTS 3. LACK OF KNOWLEDGE 4. COMMUNICATION PROBLEMS 5. INADEQUATE INSTRUCTION/PROCEDURE 6. INADEQUATE DESIGN 7. <b>NORMAL WEAR AND TEAR</b> 0. NOT APPLICABLE		1. <b>S/F BACKLOG/OPERATIONAL PRIORITY</b> 2. LACK OF MATERIAL 3. NO FORMAL TRAINING ON THIS EQUIPMENT 4. FORMAL TRAINING INADEQUATE IN THIS EQUIPMENT 5. INADEQUATE SCHOOL PRACTICAL TRAINING 6. LACK OF FACILITIES/CAPABILITIES 7. NOT AUTHORIZED FOR S/F ACCOMPLISHMENT 8. FOR S/F OVERHAUL OR AVAILABILITY WORK LIST 9. LACK OF TECHNICAL DOCUMENTATION 0. OTHER OR NOT APPLICABLE		
MAN HRS EXPENDED 2	MAN HRS REMAINING 40	COMPLETED ACTION TAKEN		S/F MANHOURS	PRIORITY			
<b>SAFETY HAZARD:</b> 1. SERIOUS - CORRECT AS SOON AS POSSIBLE 2. SERIOUS - SUSPENSION OF EQUIPMENT/SYSTEM/SPACE REQUIRED 3. SERIOUS - WAIVER OF EQUIPMENT/SYSTEM 4. <b>SAFETY ITEM - MINOR</b> 5. COMBUSTIBLE MATERIAL		1. MAINTENANCE ACTION COMPLETED: PARTS DRAWN FROM SUPPLY 2. MAINTENANCE ACTION COMPLETED: REQUIRED PARTS NOT DRAWN FROM SUPPLY 3. MAINTENANCE ACTION COMPLETED: NO PARTS REQUIRED 4. CANCELLED - DEFERRAL WILL BE REMOVED FROM CSMP 7. <b>MAINTENANCE ACTION COMPLETED: 3-M CAPABILITY UTILIZED</b> 0. NONE OF THE ABOVE, DESCRIBE IN REMARKS/DESCRIPTION SECTION			1. MANDATORY 2. ESSENTIAL 3. HIGHLY DESIRABLE 4. <b>DESIRABLE</b>			
DISCREPANCY DESCRIPTION: PER SEMAT I INSPECTION								
Port side (4-160-0-Q), Tank top under drain line from sewage transfer pump, inside coaming behind oily waste separator has surface rust and Paint separation. Approximately 6sf. U/T readings were satisfactory.								
RECOMMENDED REPAIRS:								
Needle-gun approximately 6sf of area, preserve and paint to match.								
SSPORT C221 POC: F. PRAISSMAN (757) 396-4001 EXT 2130, TPOC: T. ZWOLAK								
CSMP SUMMARY: Area specified has light corrosion and paint separation.					TEST# MRC 1102/1			
ROOT CAUSE/AMPLIFICATION: H1					STEP# 1,2,3,4 & 5			
SYSTEM LEVEL IMPACT:								
PART NO.	NOMEMCLATURE			QTY	NSN	COST		
1								
2								
3								
4								
5								
6								
FIRST CONTACT PEDRAZOLI	RATE DC1	SECOND CONTACT PEDRAZOLI		RATE DC1	PHONE 52			
ASSESSOR / ACT / TECH ID# J. LaROCK / T. ILGENFRITZ QED SYSTEMS				TD	TL	LOGISTICIAN	TSP	

**CHT ROOM (4-160-0-Q)**



PORT SIDE, TANK TOP UNDER DRAIN FROM SEWAGE TRANSFER PUMP INSIDE COAMING.  
RECOMMENDED FOR PRESERVATION AND PAINT.



PORT SIDE INSIDE COAMING

# MATERIAL ASSESSMENT FORM

ITEM NUMBER				SHIP <b>USS SIMPSON FFG 56</b>				
SYSTEM Hull Structure		ESWBS 11011	EIC A100	APL X-SYSTEMS		RIN N/L		
EQUIPMENT NAME Shell & Supporting Structure		IDENT/SERIAL N/A		LOCATION 4-140-0-Q	WORK CENTER SS03	JSN P002		
EQUIPMENT STATUS		PROBLEM STATUS		CATEGORY			EOC	
1. SAT - SATISFACTORY 2. <b>UNSAT - UNSATISFACTORY</b> 3. TNA - TEST NOT ATTEMPTED 4. TNC - TEST NOT COMPLETED 5. NOB - EQUIPMENT NOT ONBOARD		1. C - CORRECTED 2. A - AWAITING PARTS 3. T - AWAITING TECH ASSIST* 4. <b>S - SHIP'S FORCE</b> *UNCORRECTED PROBLEMS		1. S - PERSONNEL SAFETY 2. Z - EQUIPMENT SAFETY 3. <b>H - HARDWARE</b> 4. D - DOWN 5. L - LIMITED			<b>.9</b>	
TYPE/AVAILABILITY	WHEN TO BE ACCOMPLISHED	STATUS		CAUSE		DEFERRAL REASON		
1. DEPOT 2. IMA 3. TECH ASSIST 4. <b>SHIP'S FORCE</b> 5. SFWL	1. IMMEDIATELY 2. PRIOR TO NEXT UNDERWAY 3. PRIOR TO NEXT DEPLOYMENT 4. <b>DURING POST DEPLOYMENT AVAILABILITY</b>	1. <b>OPERATIONAL</b> 2. NON-OPERATIONAL 3. REDUCED CAPABILITY 0. NOT APPLICABLE		1. ABNORMAL ENVIRONMENT 2. MANUFACTURER/INSTALLATION DEFECTS 3. LACK OF KNOWLEDGE 4. COMMUNICATION PROBLEMS 5. INADEQUATE INSTRUCTION/PROCEDURE 6. INADEQUATE DESIGN 7. <b>NORMAL WEAR AND TEAR</b> 0. NOT APPLICABLE		1. <b>S/F BACKLOG/OPERATIONAL PRIORITY</b> 2. LACK OF MATERIAL 3. NO FORMAL TRAINING ON THIS EQUIPMENT 4. FORMAL TRAINING INADEQUATE IN THIS EQUIPMENT 5. INADEQUATE SCHOOL PRACTICAL TRAINING 6. LACK OF FACILITIES/CAPABILITIES 7. NOT AUTHORIZED FOR S/F ACCOMPLISHMENT 8. FOR S/F OVERHAUL OR AVAILABILITY WORK LIST 9. LACK OF TECHNICAL DOCUMENTATION 0. OTHER OR NOT APPLICABLE		
MAN HRS EXPENDED 2	MAN HRS REMAINING 80	COMPLETED ACTION TAKEN		S/F MANHOURS	PRIORITY			
<b>SAFETY HAZARD:</b> 1. SERIOUS - CORRECT AS SOON AS POSSIBLE 2. SERIOUS - SUSPENSION OF EQUIPMENT/SYSTEM/SPACE REQUIRED 3. SERIOUS - WAIVER OF EQUIPMENT/SYSTEM 4. <b>SAFETY ITEM - MINOR</b> 5. COMBUSTIBLE MATERIAL		1. MAINTENANCE ACTION COMPLETED: PARTS DRAWN FROM SUPPLY 2. MAINTENANCE ACTION COMPLETED: REQUIRED PARTS NOT DRAWN FROM SUPPLY 3. MAINTENANCE ACTION COMPLETED: NO PARTS REQUIRED 4. CANCELLED - DEFERRAL WILL BE REMOVED FROM CSMP 7. <b>MAINTENANCE ACTION COMPLETED: 3-M CAPABILITY UTILIZED</b> 0. NONE OF THE ABOVE, DESCRIBE IN REMARKS/DESCRIPTION SECTION			1. MANDATORY 2. ESSENTIAL 3. HIGHLY DESIRABLE 4. <b>DESIRABLE</b>			
DISCREPANCY DESCRIPTION: PER SEMAT I INSPECTION Port side (4-140-0-Q), bilge pocket between WF (148-164), approximately 48sf has surface rust and heavy paint separation.								
RECOMMENDED REPAIRS: Needle-gun approximately 48sf of area, preserve and paint to match.								
SSPORT C221 POC: F. PRAISSMAN (757) 396-4001 EXT 2130, TPOC: T. ZWOLAK								
CSMP SUMMARY: Area specified has surface rust and heavy paint separation.					TEST# MRC 1102/1			
ROOT CAUSE/AMPLIFICATION: H1					STEP# 1,2,3,4 & 5			
SYSTEM LEVEL IMPACT:								
PART NO.	NOMEMCLATURE			QTY	NSN		COST	
1								
2								
3								
4								
5								
6								
FIRST CONTACT KING	RATE SH2	SECOND CONTACT KING		RATE SH2	PHONE 22			
ASSESSOR / ACT / TECH ID# J. LaROCK / T. ILGENFRITZ QED SYSTEMS				TD	TL	LOGISTICIAN	TSP	

**LAUNDRY ROOM (4-140-0-Q)**



PORT SIDE, BILGE POCKET, BETWEEN WEB FRAME (148-164) HAS HEAVY RUST AND PAINT SEPARATION. AREA IS RECOMMENDED FOR PRESERVATION AND PAINT.



PORT SIDE INSIDE BILGE POCKET

# MATERIAL ASSESSMENT FORM

ITEM NUMBER				SHIP <b>USS SIMPSON FFG 56</b>				
SYSTEM Hull Structure		ESWBS 11011	EIC A100	APL X-SYSTEMS		RIN N/L		
EQUIPMENT NAME Shell & Supporting Structure		IDENT/SERIAL N/A		LOCATION 4-172-1-E	WORK CENTER EMO4	JSN P451		
EQUIPMENT STATUS		PROBLEM STATUS		CATEGORY			EOC	
1. SAT - SATISFACTORY 2. <b>UNSAT - UNSATISFACTORY</b> 3. TNA - TEST NOT ATTEMPTED 4. TNC - TEST NOT COMPLETED 5. NOB - EQUIPMENT NOT ONBOARD		1. C - CORRECTED 2. A - AWAITING PARTS 3. T - AWAITING TECH ASSIST* 4. <b>S - SHIP'S FORCE</b> *UNCORRECTED PROBLEMS		1. S - PERSONNEL SAFETY 2. Z - EQUIPMENT SAFETY 3. <b>H - HARDWARE</b> 4. D - DOWN 5. L - LIMITED			<b>.9</b>	
TYPE/AVAILABILITY	WHEN TO BE ACCOMPLISHED	STATUS	CAUSE		DEFERRAL REASON			
1. DEPOT 2. IMA 3. TECH ASSIST 4. <b>SHIP'S FORCE</b> 5. SFWL	1. IMMEDIATELY 2. PRIOR TO NEXT UNDERWAY 3. PRIOR TO NEXT DEPLOYMENT 4. <b>DURING POST DEPLOYMENT AVAILABILITY</b>	1. <b>OPERATIONAL</b> 2. NON-OPERATIONAL 3. REDUCED CAPABILITY 0. NOT APPLICABLE	1. ABNORMAL ENVIRONMENT 2. MANUFACTURER/INSTALLATION DEFECTS 3. LACK OF KNOWLEDGE 4. COMMUNICATION PROBLEMS 5. INADEQUATE INSTRUCTION/PROCEDURE 6. INADEQUATE DESIGN 7. <b>NORMAL WEAR AND TEAR</b> 0. NOT APPLICABLE		1. <b>S/F BACKLOG/OPERATIONAL PRIORITY</b> 2. LACK OF MATERIAL 3. NO FORMAL TRAINING ON THIS EQUIPMENT 4. FORMAL TRAINING INADEQUATE IN THIS EQUIPMENT 5. INADEQUATE SCHOOL PRACTICAL TRAINING 6. LACK OF FACILITIES/CAPABILITIES 7. NOT AUTHORIZED FOR S/F ACCOMPLISHMENT 8. FOR S/F OVERHAUL OR AVAILABILITY WORK LIST 9. LACK OF TECHNICAL DOCUMENTATION 0. OTHER OR NOT APPLICABLE			
MAN HRS EXPENDED 2	MAN HRS REMAINING 120	COMPLETED ACTION TAKEN		S/F MANHOURS	PRIORITY			
<b>SAFETY HAZARD:</b> 1. SERIOUS - CORRECT AS SOON AS POSSIBLE 2. SERIOUS - SUSPENSION OF EQUIPMENT/SYSTEM/SPACE REQUIRED 3. SERIOUS - WAIVER OF EQUIPMENT/SYSTEM 4. <b>SAFETY ITEM - MINOR</b> 5. COMBUSTIBLE MATERIAL		1. MAINTENANCE ACTION COMPLETED: PARTS DRAWN FROM SUPPLY 2. MAINTENANCE ACTION COMPLETED: REQUIRED PARTS NOT DRAWN FROM SUPPLY 3. MAINTENANCE ACTION COMPLETED: NO PARTS REQUIRED 4. CANCELLED - DEFERRAL WILL BE REMOVED FROM CSMP 7. <b>MAINTENANCE ACTION COMPLETED: 3-M CAPABILITY UTILIZED</b> 0. NONE OF THE ABOVE, DESCRIBE IN REMARKS/DESCRIPTION SECTION			1. MANDATORY 2. ESSENTIAL 3. HIGHLY DESIRABLE 4. <b>DESIRABLE</b>			
DISCREPANCY DESCRIPTION: PER SEMAT I INSPECTION								
STBD side, Fire pump room (4-172-1-E), entire bilge pocket area approximately 24sf of rust and paint separation, pump foundation and area Underneath approximately 30sf of rust and paint separation.								
RECOMMENDED REPAIRS: Needle-gun approximately 54sf of entire area, preserve and paint to match.								
SSPORT C221 POC: F. PRAISSMAN (757) 396-4001 EXT 2130, TPOC: T. ZWOLAK								
CSMP SUMMARY: Area specified has rust and paint separation.					TEST# MRC 1102/1			
ROOT CAUSE/AMPLIFICATION: H1					STEP# 1,2,3,4 & 5			
SYSTEM LEVEL IMPACT:								
PART NO.	NOMEMCLATURE			QTY	NSN	COST		
1								
2								
3								
4								
5								
6								
FIRST CONTACT PEDRAZOLI	RATE DC1	SECOND CONTACT PEDRAZOLI		RATE DC1	PHONE 52			
ASSESSOR / ACT / TECH ID# J. LaROCK / T. ILGENFRITZ QED SYSTEMS				TD	TL	LOGISTICIAN	TSP	

**FIRE PUMP ROOM (4-172-1-E)**



STBD SIDE, BILGE POCKET,  
RECOMMENDED FOR PRESERVATION AND PAINT.



STBD SIDE INSIDE BILGE POCKET

# MATERIAL ASSESSMENT FORM

ITEM NUMBER				SHIP <b>USS SIMPSON FFG 56</b>				
SYSTEM Hull Structure		ESWSB 11011	EIC A100	APL X-SYSTEMS		RIN N/L		
EQUIPMENT NAME Shell & Supporting Structure		IDENT/SERIAL N/A		LOCATION 3-84-0-E	WORK CENTER EMO2	JSN P049		
EQUIPMENT STATUS		PROBLEM STATUS		CATEGORY			EOC	
1. SAT - SATISFACTORY 2. <b>UNSAT - UNSATISFACTORY</b> 3. TNA - TEST NOT ATTEMPTED 4. TNC - TEST NOT COMPLETED 5. NOB - EQUIPMENT NOT ONBOARD		1. C - CORRECTED 2. A - AWAITING PARTS 3. <b>T - AWAITING TECH ASSIST*</b> 4. S - SHIP'S FORCE *UNCORRECTED PROBLEMS		1. S - PERSONNEL SAFETY 2. Z - EQUIPMENT SAFETY 3. <b>H - HARDWARE</b> 4. D - DOWN 5. L - LIMITED			<b>.9</b>	
TYPE/AVAILABILITY	WHEN TO BE ACCOMPLISHED	STATUS		CAUSE		DEFERRAL REASON		
1. DEPOT 2. IMA 3. <b>TECH ASSIST</b> 4. SHIP'S FORCE 5. SFWL	1. IMMEDIATELY 2. PRIOR TO NEXT UNDERWAY 3. PRIOR TO NEXT DEPLOYMENT 4. <b>DURING POST DEPLOYMENT AVAILABILITY</b>	1. <b>OPERATIONAL</b> 2. NON-OPERATIONAL 3. REDUCED CAPABILITY 0. NOT APPLICABLE		1. ABNORMAL ENVIRONMENT 2. MANUFACTURER/INSTALLATION DEFECTS 3. LACK OF KNOWLEDGE 4. COMMUNICATION PROBLEMS 5. INADEQUATE INSTRUCTION/PROCEDURE 6. INADEQUATE DESIGN 7. <b>NORMAL WEAR AND TEAR</b> 0. NOT APPLICABLE		1. S/F BACKLOG/OPERATIONAL PRIORITY 2. LACK OF MATERIAL 3. NO FORMAL TRAINING ON THIS EQUIPMENT 4. FORMAL TRAINING INADEQUATE IN THIS EQUIPMENT 5. INADEQUATE SCHOOL PRACTICAL TRAINING 6. LACK OF FACILITIES/CAPABILITIES 7. <b>NOT AUTHORIZED FOR S/F ACCOMPLISHMENT</b> 8. FOR S/F OVERHAUL OR AVAILABILITY WORK LIST 9. LACK OF TECHNICAL DOCUMENTATION 0. OTHER OR NOT APPLICABLE		
MAN HRS EXPENDED 2	MAN HRS REMAINING 1000	COMPLETED ACTION TAKEN		S/F MANHOURS	PRIORITY			
<b>SAFETY HAZARD:</b> 1. <b>SERIOUS - CORRECT AS SOON AS POSSIBLE</b> 2. SERIOUS - SUSPENSION OF EQUIPMENT/SYSTEM/SPACE REQUIRED 3. SERIOUS - WAIVER OF EQUIPMENT/SYSTEM 4. SAFETY ITEM - MINOR 5. COMBUSTIBLE MATERIAL		1. MAINTENANCE ACTION COMPLETED: PARTS DRAWN FROM SUPPLY 2. MAINTENANCE ACTION COMPLETED: REQUIRED PARTS NOT DRAWN FROM SUPPLY 3. MAINTENANCE ACTION COMPLETED: NO PARTS REQUIRED 4. CANCELLED - DEFERRAL WILL BE REMOVED FROM CSMP 7. <b>MAINTENANCE ACTION COMPLETED: 3-M CAPABILITY UTILIZED</b> 0. NONE OF THE ABOVE, DESCRIBE IN REMARKS/DESCRIPTION SECTION			1. MANDATORY 2. ESSENTIAL 3. <b>HIGHLY DESIRABLE</b> 4. DESIRABLE			
DISCREPANCY DESCRIPTION: PER SEMAT I INSPECTION								
Port side (3-84-0-E), deck under air conditioning condenser unit and BHD (100), U/T readings taken between foundation and BHD (100) and Between foundation and shell plate were unsatisfactory with readings at <b>.194, .151, .115, .177, .120, .154 and</b>								
Were taken 2" above the deck, behind foundation were <b>.139, .180, .177, .176, .239 and .254.</b>								
<b>.108.</b> Readings on BHD (100) The minimum is <b>.213</b>								
RECOMMEND REPAIR Activity: Cut out approximately 30sf of tank top/deck, replace with .250 steel. Cut out approximately 10sf of BHD (100), from deck up 12", Replace with 250 steel, preserve and paint to match.								
FTSC is to conduct risk assessment and make final repair recommendations.								
SSPORT C221 POC: F. PRAISSMAN (757) 396-4001 EXT 2130, TPOC: T. ZWOLAK								
CSMP SUMMARY: Deck under A/C foundation and BHD (100) severely deteriorated.					TEST# MRC 1102/1			
ROOT CAUSE/AMPLIFICATION: H1					STEP# 1,2,3,4 & 5			
SYSTEM LEVEL IMPACT:								
PART NO.	NOMEMCLATURE			QTY	NSN		COST	
1								
2								
3								
4								
5								
6								
FIRST CONTACT LARSEN	RATE EN3	SECOND CONTACT LARSEN		RATE EN3	PHONE 22			
ASSESSOR / ACT / TECH ID# J. LaROCK / T. ILGENFRITZ QED SYSTEMS				TD	TL	LOGISTICIAN	TSP	

**A/C MACHINERY ROOM (3-84-0-E)**



PORT SIDE, A/C CONDENSER FOUNDATION AT BHD 100



HEAVY PITTING NOTED INSIDE FOUNDATION FOR THE A/C CONDENSER UNIT.  
U/T READINGS ARE NOTED ON DRAWING.

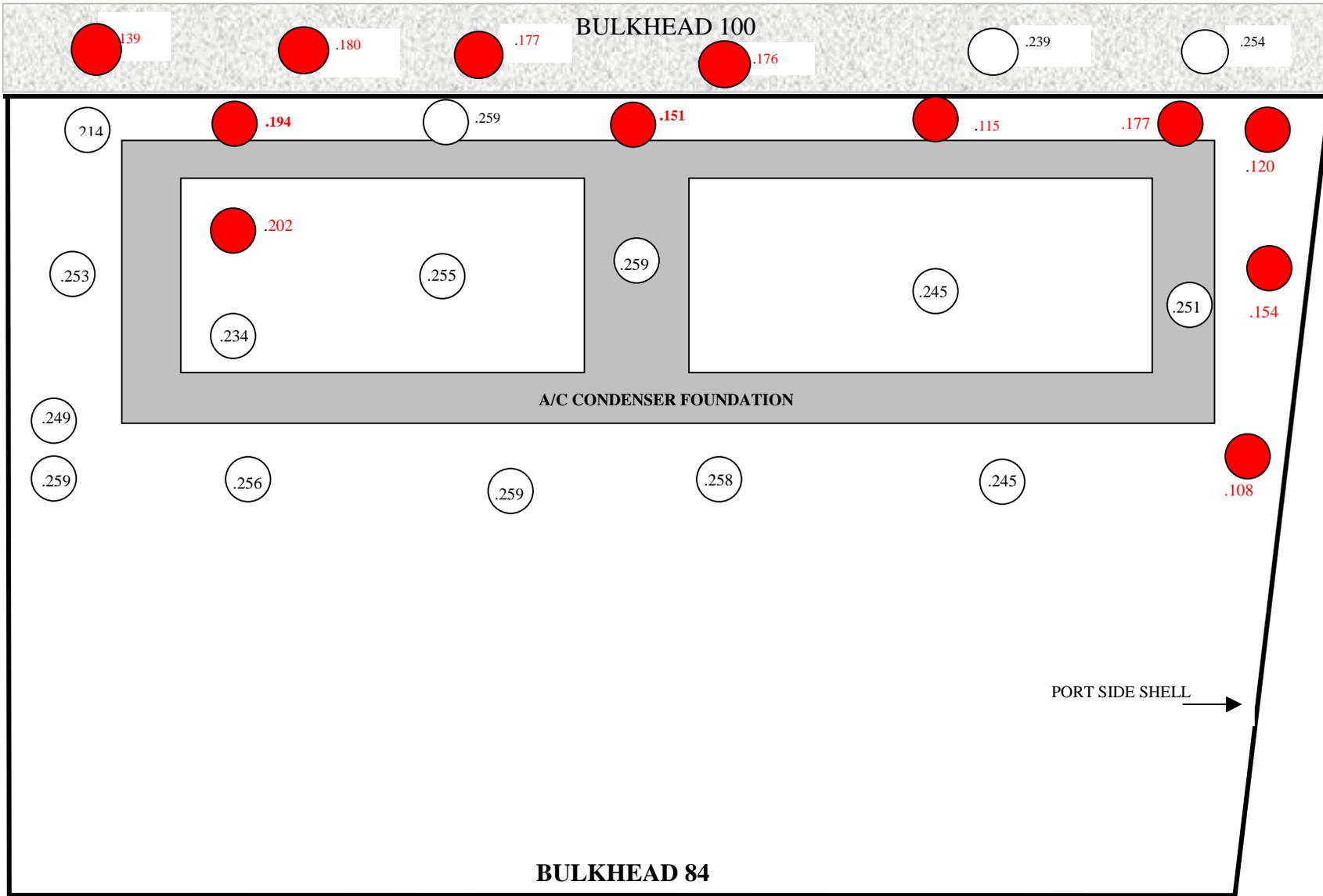
**A/C MACHINERY ROOM (3-84-0-E)**



PORT SIDE, BETWEEN A/C CONDENSER FOUNDATION AND BHD 100  
U/T READINGS ARE ON DRAWING



DECK AND BHD 100  
U/T READINGS ARE NOTED ON DRAWING.



USS SIMPSON (FFG 56)  
 A/C MACHINERY ROOM (3-84-0-E)  
 Top view

J. LaROCK / T. ILGENFRITZ  
 QED SYSTEMS INC.  
 06/08/01

# MATERIAL ASSESSMENT FORM

ITEM NUMBER				SHIP <b>USS SIMPSON FFG 56</b>				
SYSTEM Hull Structure		ESWBS 11011	EIC A100	APL X-SYSTEMS		RIN N/L		
EQUIPMENT NAME Shell & Supporting Structure		IDENT/SERIAL N/A		LOCATION 4-48-1-Q	WORK CENTER EMO4		JSN P487	
EQUIPMENT STATUS		PROBLEM STATUS		CATEGORY			EOC	
1. SAT - SATISFACTORY 2. <b>UNSAT - UNSATISFACTORY</b> 3. TNA - TEST NOT ATTEMPTED 4. TNC - TEST NOT COMPLETED 5. NOB - EQUIPMENT NOT ONBOARD		1. C - CORRECTED 2. A - AWAITING PARTS 3. T - AWAITING TECH ASSIST* 4. <b>S - SHIP'S FORCE</b> *UNCORRECTED PROBLEMS		1. S - PERSONNEL SAFETY 2. Z - EQUIPMENT SAFETY 3. <b>H - HARDWARE</b> 4. D - DOWN 5. L - LIMITED			<b>.9</b>	
TYPE/AVAILABILITY	WHEN TO BE ACCOMPLISHED	STATUS		CAUSE		DEFERRAL REASON		
1. DEPOT 2. IMA 3. TECH ASSIST 4. <b>SHIP'S FORCE</b> 5. SFWL	1. IMMEDIATELY 2. PRIOR TO NEXT UNDERWAY 3. PRIOR TO NEXT DEPLOYMENT 4. <b>DURING POST DEPLOYMENT AVAILABILITY</b>	1. <b>OPERATIONAL</b> 2. NON-OPERATIONAL 3. REDUCED CAPABILITY 0. NOT APPLICABLE		1. ABNORMAL ENVIRONMENT 2. MANUFACTURER/INSTALLATION DEFECTS 3. LACK OF KNOWLEDGE 4. COMMUNICATION PROBLEMS 5. INADEQUATE INSTRUCTION/PROCEDURE 6. INADEQUATE DESIGN 7. <b>NORMAL WEAR AND TEAR</b> 0. NOT APPLICABLE		1. <b>S/F BACKLOG/OPERATIONAL PRIORITY</b> 2. LACK OF MATERIAL 3. NO FORMAL TRAINING ON THIS EQUIPMENT 4. FORMAL TRAINING INADEQUATE IN THIS EQUIPMENT 5. INADEQUATE SCHOOL PRACTICAL TRAINING 6. LACK OF FACILITIES/CAPABILITIES 7. NOT AUTHORIZED FOR S/F ACCOMPLISHMENT 8. FOR S/F OVERHAUL OR AVAILABILITY WORK LIST 9. LACK OF TECHNICAL DOCUMENTATION 0. OTHER OR NOT APPLICABLE		
MAN HRS EXPENDED 2	MAN HRS REMAINING 100		COMPLETED ACTION TAKEN		S/F MANHOURS	PRIORITY		
<b>SAFETY HAZARD:</b> 1. SERIOUS - CORRECT AS SOON AS POSSIBLE 2. SERIOUS - SUSPENSION OF EQUIPMENT/SYSTEM/SPACE REQUIRED 3. SERIOUS - WAIVER OF EQUIPMENT/SYSTEM 4. <b>SAFETY ITEM - MINOR</b> 5. COMBUSTIBLE MATERIAL			1. MAINTENANCE ACTION COMPLETED: PARTS DRAWN FROM SUPPLY 2. MAINTENANCE ACTION COMPLETED: REQUIRED PARTS NOT DRAWN FROM SUPPLY 3. MAINTENANCE ACTION COMPLETED: NO PARTS REQUIRED 4. CANCELLED - DEFERRAL WILL BE REMOVED FROM CSMP 7. <b>MAINTENANCE ACTION COMPLETED: 3-M CAPABILITY UTILIZED</b> 0. NONE OF THE ABOVE, DESCRIBE IN REMARKS/DESCRIPTION SECTION			1. MANDATORY 2. ESSENTIAL 3. HIGHLY DESIRABLE 4. <b>DESIRABLE</b>		
DISCREPANCY DESCRIPTION: PER SEMAT I INSPECTION								
STBD side (4-48-1-Q), Found approximately 48sf of saturated lagging, from the deck to the second stringer. BHD (56) showed light corrosion								
And paint separation with minor thinning. Random U/T readings in a 1sf area were .220, .235, .236, .227.								
RECOMMENDED REPAIRS:								
Remove approximately 48sf of lagging. Needle-gun, preserve and paint areas exposed by removals, replace with 2" hull board. Clean approx 4sf of BHD (56), preserve, paint to match area.								
SSPORT C221 POC: F. PRAISSMAN (757) 396-4001 EXT 2130, TPOC: T. ZWOLAK								
CSMP SUMMARY: saturated lagging, mild corrosion and paint separation.					TEST# MRC 1102/1			
ROOT CAUSE/AMPLIFICATION: H1					STEP# 1,2,3,4 & 5			
SYSTEM LEVEL IMPACT:								
PART NO.	NOMEMCLATURE			QTY	NSN		COST	
1								
2								
3								
4								
5								
6								
FIRST CONTACT HOPPER		RATE STG3	SECOND CONTACT HOPPER		RATE STG3	PHONE 28/700/720		
ASSESSOR / ACT / TECH ID# J. LaROCK / T. ILGENFRITZ QED SYSTEMS				TD	TL	LOGISTICIAN	TSP	

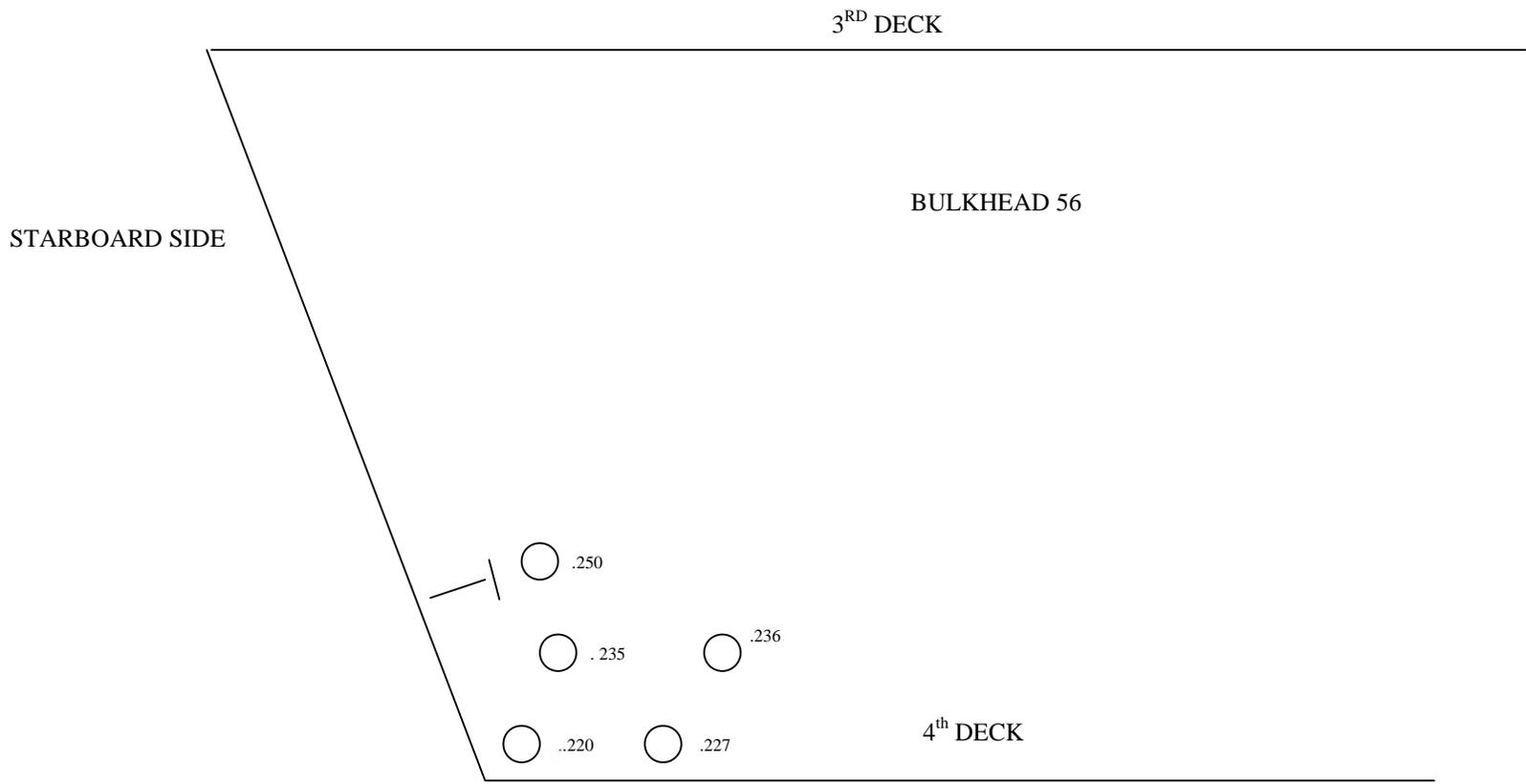
**SONAR COOLING ROOM (4-48-1-Q)**



STBD SIDE, LOOKING AFT AT BHD 56 LIGHT THINNING WAS NOTED.  
U/T READINGS ARE SHOWN IN DRAWING.



DAMAGED LAGGING ON SHELL PLATE  
REPLACEMENT RECOMMENDED.



USS SIMPSON FFG 56  
 SONAR COOLING ROOM (4-48-1-Q)  
 Starboard side looking aft

J.LAROCK / T. ILGENFRITZ  
 QED SYSTEMS INC.  
 06/10/01

<b>SPACE NUMBER</b>	<b>SPACE ASSESSABLE</b>	<b>LAGGING INSTALLED ON BHD</b>	<b>IS THE SPACE INSPECTED DURING SEMAT 1</b>	<b>HISTORICAL HOT SPOT</b>	<b>COMMENTS</b>
<b>3RD DECK</b>					
3-20-0-Q	Y	N	Y		SPACE REQUIRES GAS FREE
3-32-2-A	Y	Y	N		ACCESSIBLE HIDDEN LAGGED 100% (100% OF SHELL ASSESSABLE)
3-32-1-K	Y	Y	N		ACCESSIBLE HIDDEN LAGGED 100% (100% OF SHELL ASSESSABLE)
3-36-2-T	Y	Y	N		ACCESSIBLE HIDDEN LAGGED 100% (100% OF SHELL ASSESSABLE)
3-40-2-A	NOT DURING SHIPCHECK		N		
3-46-1-A	NOT DURING SHIPCHECK		N		
3-48-2-A	NOT DURING SHIPCHECK		N		
3-53-2-A	NOT DURING SHIPCHECK		N		
3-36-1-A	NOT DURING SHIPCHECK		N		
3-56-0-A	NOT DURING SHIPCHECK		N		
3-64-2-V	Y	N	Y		SPACE REQUIRES GAS FREE
3-64-1-V	Y	N	Y		SPACE REQUIRES GAS FREE

SPACE NUMBER	SPACE ASSESSABLE	LAGGING INSTALLED ON BHD	IS THE SPACE INSPECTED DURING SEMAT 1	HISTORICAL HOT SPOT	COMMENTS
<b>3RD DECK</b>					
3-84-0-E	Y	Y	N	FRS 94-100 STBD SIDE SHELL, DECK TO OVERHEAD  FR 95, PORT SIDE, ON TANK TOP	ACCESSIBLE HIDDEN LAGGED 100% (100% OF PORT SIDE SHELL ASSESSABLE)  ACCESSIBLE HIDDEN LAGGED 100% (NOT ACCESSIBLE AREA - STBD FWD BHD 84 TO FRAME 93, 5'-6" OFF DECK ELECT PANELS)
3-93-1-Q	Y	Y	N		ACCESSIBLE HIDDEN LAGGED 100% (100% OF PORT SIDE SHELL ASSESSABLE)
3-100-0-L	Y	Y	N		ACCESSIBLE HIDDEN LAGGED 100% (NOT ACCESSIBLE AREA PORT AND STBD FROM FRAME 115 TO AFT BHD 140, 6' 3" OFF DECK - BUNKS)
3-100-1-L	Y	Y	N		LAGGING 100% AND SS 50% (SS INSTALLED 49" OFF DECK, FRAME 100 TO 113)
3-113-0-L	Y	Y	N	FR 113-124, STBD SIDE OF SHELL, DECK TO OVERHEAD	LAGGING 100% AND SS 50% (SS INSTALLED 49" OFF DECK, FRAME 113 TO 123)
3-140-2-L	Y	Y	N		LAGGING 100% AND SS 50% (SS INSTALLED 49" OFF DECK, FRAME 140 TO 155)

SPACE NUMBER	SPACE ASSESSABLE	LAGGING INSTALLED ON BHD	IS THE SPACE INSPECTED DURING SEMAT 1	HISTORICAL HOT SPOT	COMMENTS
<b>3RD DECK</b>					
3-140-1-L	Y	Y	N		LAGGING 100% AND SS 50% (SS INSTALLED 49" OFF DECK, FRAMES 140 TO 155)
3-144-0-L	Y	Y	N		ACCESSIBLE HIDDEN LAGGED 100% (NOT ACCESSIBLE AREA, PORT & STBD FRAME 165 TO 180, 6'3" OFF DECK BUNKS)
3-156-2-Q	Y	Y	N		ACCESSIBLE HIDDEN LAGGED 100% (100% OF SHELL ASSESSABLE)
3-154-1-Q	Y	Y	N		ACCESSIBLE HIDDEN LAGGED 100% (100% OF SHELL ASSESSABLE)
3-180-2-C	Y	Y	N		ACCESSIBLE HIDDEN LAGGED 100% (100% OF SHELL ASSESSABLE)
3-180-5-A	Y	Y	N		ACCESSIBLE HIDDEN LAGGED 100% (100% OF SHELL ASSESSABLE)
3-196-2-A	Y	Y	N		ACCESSIBLE HIDDEN LAGGED 100% (100% OF SHELL ASSESSABLE)
3-236-1-F	Y	N	Y		SPACE REQUIRES GAS FREE
3-236-2-F	Y	N	Y		SPACE REQUIRES GAS FREE
3-240-1-F	Y	N	Y		SPACE REQUIRES GAS FREE

SPACE NUMBER	SPACE ASSESSABLE	LAGGING INSTALLED ON BHD	IS THE SPACE INSPECTED DURING SEMAT 1	HISTORICAL HOT SPOT	COMMENTS
<b>3RD DECK</b>					
3-240-2-F	Y	N	Y		SPACE REQUIRES GAS FREE
3-272-2-F	Y	N	Y		SPACE REQUIRES GAS FREE
3-272-1-F	Y	N	Y		SPACE REQUIRES GAS FREE
3-278-1-F	Y	N	Y		SPACE REQUIRES GAS FREE
3-278-2-F	Y	N	Y		SPACE REQUIRES GAS FREE
3-286-2-F	Y	N	Y		SPACE REQUIRES GAS FREE
3-286-1-F	Y	N	Y		SPACE REQUIRES GAS FREE
3-292-8-F	Y	N	Y		SPACE REQUIRES GAS FREE
3-316-1-J	Y	N	Y		SPACE REQUIRES GAS FREE
3-322-1-J	Y	N	Y		SPACE REQUIRES GAS FREE
3-328-0-A	Y	Y	N		ACCESSIBLE HIDDEN LAGGED 100% (100% OF SHELL ASSESSABLE)

SPACE NUMBER	SPACE ASSESSABLE	LAGGING INSTALLED ON BHD	IS THE SPACE INSPECTED DURING SEMAT 1	HISTORICAL HOT SPOT	COMMENTS
<b>4TH DECK</b>					
4-H-0-V	Y	N	Y		SPACE REQUIRES GAS FREE
4-20-0-W	Y	N	Y		SPACE REQUIRES GAS FREE
4-27-0-V	N	N	Y		SPACE REQUIRES GAS FREE FR 27 FR 32 P/S FROM 3' UP 26' 4 1/16
4-32-0-Q	NOT DURING SHIPCHECK		N		
4-48-2-L	Y	Y	N		ACCESSIBLE HIDDEN LAGGED 100% (100% OF SHELL ASSESSABLE)
4-48-1-Q	Y	Y	N	FR 49-55 STBD SIDE OF SHELL, 4'-0' OFF DECK	ACCESSIBLE HIDDEN LAGGED 100% (NOT ACCESSIBLE AREA FRAME 50 TO AFT BHD 56, 4'-0' OFF DECK HEATEX & PPG)
4-56-2-A	Y	Y	N		ACCESSIBLE HIDDEN LAGGED 100% (100% OF SHELL ASSESSABLE)
4-56-0-M	NOT DURING SHIPCHECK		N		ACCESSIBLE HIDDEN LAGGED 100% (100% OF SHELL ASSESSABLE)

SPACE NUMBER	SPACE ASSESSABLE	LAGGING INSTALLED ON BHD	IS THE SPACE INSPECTED DURING SEMAT 1	HISTORICAL HOT SPOT	COMMENTS
<b>4TH DECK</b>					
4-64-0-Q	NOT DURING SHIPCHECK		N		
4-100-0-E	Y	Y	N	FR 100-101 STBD SIDE OF SHELL 6'BELOW GRATING	ACCESSIBLE HIDDEN LAGGED 100% (100% OF PORT & STBD SHELL ASSESSABLE)
4-100-0-Q	Y	Y	N	TANK TOP FOR 5-140 & 5-164-2F	STAINLESS STEEL 100% (100% OF PORT SIDE SHELL ASSESSABLE)
4-140-3-A	Y	N	N		ACCESSIBLE HIDDEN LAGGED 100%(100% OF SHELL ASSESSABLE)
4-152-1-A	Y	N	N		ACCESSIBLE HIDDEN LAGGED 100% (100% OF SHELL ASSESSABLE)
4-160-0-Q	Y	N	N		100% OF PORT AND STBD SHELL ASSESSABLE
4-172-1-E	Y	Y	N	FR 174-180 STBD OF SHELL 6" BELOW GRATING BETWEEN L7-L9	100% OF PORT & STBD SHELL ASSESSABLE

SPACE NUMBER	SPACE ASSESSABLE	LAGGING INSTALLED ON BHD	IS THE SPACE INSPECTED DURING SEMAT 1	HISTORICAL HOT SPOT	COMMENTS
<b>5TH DECK</b>					
5-FF-0-V	N	N	N		INACCESSIBLE VOID FR 18 - FR 32, P/S. FROM STEM UP 4FT. FR 5 - FR 18, P/S. FROM STEM UP ~6FT; FR E - 12, P/S. FROM STEM UP ~10FT
5-32-0-W	Y	N	Y		SPACE REQUIRES GAS FREE
5-51-0-Q	Y	N	N		100% OF PORT & STBD SHELL ASSESSABLE
5-56-0-F	Y	N	Y		SPACE REQUIRES GAS FREE
5-64-0-F	Y	N	Y		SPACE REQUIRES GAS FREE
5-84-2-F	Y	N	Y		SPACE REQUIRES GAS FREE
5-84-1-F	Y	N	Y		SPACE REQUIRES GAS FREE
5-100-2-W	Y	Y	N		ACCESSIBLE HIDDEN LAGGED 100% (100% OF SHELL ASSESSABLE)
5-100-1-W	Y	Y	N		ACCESSIBLE HIDDEN LAGGED 100% (100% OF SHELL ASSESSABLE)
5-100-4-F	Y	N	Y		SPACE REQUIRES GAS FREE
5-100-3-F	Y	N	Y		SPACE REQUIRES GAS FREE
5-116-2-F	Y	N	Y		SPACE REQUIRES GAS FREE
5-116-1-F	Y	N	Y		SPACE REQUIRES GAS FREE

SPACE NUMBER	SPACE ASSESSABLE	LAGGING INSTALLED ON BHD	IS THE SPACE INSPECTED DURING SEMAT 1	HISTORICAL HOT SPOT	COMMENTS
<b>5TH DECK</b>					
51/2-116-0-V	N	N	Y		INACCESSIBLE VOID FR 116 - FR 132, P/S FROM KEEL UP ~4FT
5-132-0-F	Y	N	Y		SPACE REQUIRES GAS FREE
5-137-1-T	Y	N	Y		SPACE REQUIRES GAS FREE
5-140-2-F	Y	N	Y		SPACE REQUIRES GAS FREE
5-140-1-F	Y	N	Y		SPACE REQUIRES GAS FREE
5-164-0-F	Y	N	Y		SPACE REQUIRES GAS FREE
5-164-2-F	Y	N	Y		SPACE REQUIRES GAS FREE
5-164-1-F	Y	N	Y		SPACE REQUIRES GAS FREE
5-170-0-F	Y	N	Y		SPACE REQUIRES GAS FREE
5-180-0-E	Y	Y	N	FRAME 180, PORT 15'-0" OFF CL, 8' SQFT	SHELL NON-ASSESSABLE ON PORT SIDE FRAME 180 TO 195, FRAME 204 TO 212, OVERHEAD TO BILGE, TANK & FND
5-180-01-E	Y	Y	N		100% OF STBD SHELL ASSESSABLE
5-192-2-F	Y	N	Y		SPACE REQUIRES GAS FREE
5-201-1-F	Y	N	Y		SPACE REQUIRES GAS FREE
5-201-3-F	Y	N	Y		SPACE REQUIRES GAS FREE

SPACE NUMBER	SPACE ASSESSABLE	LAGGING INSTALLED ON BHD	IS THE SPACE INSPECTED DURING SEMAT 1	HISTORICAL HOT SPOT	COMMENTS
<b>5TH DECK</b>					
5-204-2-F	Y	N	Y		SPACE REQUIRES GAS FREE
5-204-1-F	Y	N	Y		SPACE REQUIRES GAS FREE
5-212-0-E(UL)	Y	Y	N	FRS 215-220 PORT SIDE OF SHELL, U/L BETWEEN L13-L14	ACCESSIBLE HIDDEN LAGGED UPPER LEVEL. (NOT ACCESSIBLE AREA, PORT SIDE FRAME 220 TO 224, 5'-0" OFF U/L GRATING VOLTAGE REGULATOR)
5-212-0-E(LL)					ASSESSABLE HIDDEN, LAGGED LOWER LEVEL, ACCESSIBLE AREA PORT FRAME 212 TO 228, L10 TO BILGE AREA, STBD FRAME 212 TO 228 L8 TO BILGE AREA. (NOT ACCESSIBLE AREA - PORT & STBD FRAME 228 TO 250, FROM U/L TO BILGE AREA FUEL OIL HEATER)
5-220-2-W	Y	N	Y		SPACE REQUIRES GAS FREE
5-222-1-F	Y	N	Y		SPACE REQUIRES GAS FREE

SPACE NUMBER	SPACE ASSESSABLE	LAGGING INSTALLED ON BHD	IS THE SPACE INSPECTED DURING SEMAT 1	HISTORICAL HOT SPOT	COMMENTS
<b>5TH DECK</b>					
5-226-2-E	N	Y	N		SHELL NON-ASSESSABLE FRAME 225 TO 229, 4'-2" OFF U/L GRATING, FRAME 235 TO 250, 6'-3" OFF U/L GRATING - AIR FLASK, MANIFOLD & FUEL TANK
5-226-1-E	N	Y	N		SHELL NON-ASSESSABLE FRAME 225 TO 229, 4'-2" OFF U/L GRATING, FRAME 235 TO 250, 6'-3" OFF U/L GRATING, AIR FLASK, MANIFOLD & FUEL TANK
5-250-0-E	Y	Y	N	FR 271-272 PORT SIDE OF SHELL TOP OF L0 TANK TOP	LAGGED EXCEPT FOR BILGE AREA, AT 18" ABOVE GRATING INTO BILGE AREA, (SHELL NON-ASSESSABLE ON PORT 1'-0" AFT OF FRAME 270 TO FRAME 292, FROM 5'-0" OFF GRATING, STBD FRAME 265 TO 270 FROM 1'-0" FROM OVERHEAD TO 6" BELOW GRATING, 14" AFT OF FRAME 270 TO 273, 5'-0" OFF GRATING POWER PANELS)

SPACE NUMBER	SPACE ASSESSABLE	LAGGING INSTALLED ON BHD	IS THE SPACE INSPECTED DURING SEMAT 1	HISTORICAL HOT SPOT	COMMENTS
<b>5TH DECK</b>					
5-250-0-E (CONT'D)	Y	Y	N	FR 250 PORT SIDE OF SHELL, BETWEEN L9-L12	
5-250-0-E (CONT'D)	Y	Y	N	FR 258-264 STBD SIDE OF SHELL, BETWEEN L9-L12	
5-250-1-F	Y	N	Y		SPACE REQUIRES GAS FREE
5-250-2-F	Y	N	Y		SPACE REQUIRES GAS FREE
5-270-0-F	Y	N	Y		SPACE REQUIRES GAS FREE
5-276-0-F	Y	N	Y		SPACE REQUIRES GAS FREE
5-290-2-F	Y	N	Y		SPACE REQUIRES GAS FREE
5-292-0-E	Y	Y	N	FR 316-328 PORT SIDE OF SHELL BETWEEN L5-L9	ACCESSIBLE HIDDEN LAGGED, ASSESSABLE AREA PORT FRAME 292 TO 318, DK GRATING TO OVERHEAD (SHELL NON-ASSESSABLE ON PORT FRAME 318 TO 328, 2'-0" FROM GRATING TO OVERHEAD OF SPACE, STBD FRAME 292 TO 300, 3'-0" OFF GRATING

SPACE NUMBER	SPACE ASSESSABLE	LAGGING INSTALLED ON BHD	IS THE SPACE INSPECTED DURING SEMAT 1	HISTORICAL HOT SPOT	COMMENTS
<b>5TH DECK</b>					
5-292-0-E (CONT'D)	Y	Y	N	FR 316-328 PORT SIDE OF SHELL BETWEEN L5- L9	TO OVERHEAD, FRAMES 305 TO 308, 3'-0" OFF GRATING TO OVERHEAD, FRAME 313 TO 328, 6'- 3" OFF GRATING & POWER PANELS
5-292-0-E (CONT'D)				FR 292-328 PORT SIDE OF SHELL BETWEEN L1- L3	
5-292-1-W	Y	N	Y		SPACE REQUIRES GAS FREE
5-292-2-W	Y	N	Y		SPACE REQUIRES GAS FREE
5-292-3-W	Y	N	Y		SPACE REQUIRES GAS FREE
5-308-2-W	Y	N	Y		SPACE REQUIRES GAS FREE
5-308-1-W	Y	N	Y		SPACE REQUIRES GAS FREE
5-328-0-J	Y	N	Y		SPACE REQUIRES GAS FREE
5-328-2-W	Y	N	Y		SPACE REQUIRES GAS FREE

SPACE NUMBER	SPACE ASSESSABLE	LAGGING INSTALLED ON BHD	IS THE SPACE INSPECTED DURING SEMAT 1	HISTORICAL HOT SPOT	COMMENTS
<b>5TH DECK</b>					
5-328-1-W	Y	N	Y		SPACE REQUIRES GAS FREE
5-344-0-J	Y	N	Y		SPACE REQUIRES GAS FREE
5-368-0-V	Y	N	Y		SPACE REQUIRES GAS FREE
5-386-0-V	N	N			INACCESSIBLE VOID FRAME 386 TO 392, P/S, FROM KEEL UP ~2FT; FRAME 392 TO 400, S, FROM KEEL UP ~2FT; FRAME 400 TO 408 P/S FROM KEEL UP ~2FT
5-392-0-V	Y	N	Y		SPACE REQUIRES GAS FREE
5-368-01-E	Y	Y	N		ACCESSIBLE HIDDEN LAGGED 100% (100% OF SHELL ASSESSABLE)
5-376-1-A	Y	Y	N		ACCESSIBLE HIDDEN LAGGED 100% (100% OF SHELL ASSESSABLE)